



# Memorandum

Planning Division  
Community & Neighborhood Department

**To:** Planning Commission

**From:** Nannette Larsen, Principal Planner

**Date:** September 30, 2020

**Re:** Work Session for 800 S + State Street – Design Review  
Petition PLNPCM2020-00439

## **800 S + STATE STREET – DESIGN REVIEW**

**Property Address:** 754 South State Street  
**Parcel IDs:** 16-07-103-022, 16-07-103-017, 16-07-103-010  
**Zoning District:** D-2 (Downtown Support)  
**Master Plan:** Downtown – South State District

### **PROJECT OVERVIEW**

#### **ATTACHMENTS:**

- A. Standards for Design Review
- B. Site Plan
- C. Building Elevations
- D. Site Photos
- E. Applicant's Plans

<b>Design Standards Met</b>	ground floor use, building materials, glazing, blank wall length, lighting, screening,
<b>Design Standards Modified</b>	building entrances, building length façade, building height.
<b>Proposed Height</b>	123' (5' parapet walls)
<b>Building Height by Right</b>	65'
<b>Building Length Proposed</b>	373'
<b>Building Length by Right</b>	200'
<b>Ground Floor Uses</b>	retail/commercial
<b>Upper Floor Uses</b>	300 residential units
<b>Ground Floor Glazing</b>	60%-74%
<b>Upper Floor Glazing</b>	36%-39%

#### **ACTION REQUESTED**

Salt Lake City has received a request from Aabir Malik with Colmena Group, for approval of modifications to the required Design Standards in the Downtown Support (D-2) District to construct a new 11-story mixed-use development on the corner of 800 South and State Street. The standards proposed to be modified for the 800 S + State Street project include:

- Additional building height;
- Operable building entrances at least every 50' on street facing building facades;
- And, the maximum length of a street facing façade of 200'.

The proposed 800 South + State Street project meets the following design standards:

- Ground floor use other than parking along at least 80% of the street facing building façade;
- Durable building materials which cover at least 80% on the ground floor and 50% on the upper floors;
- Glazing covering at least 40% of the ground floor and 25% on the upper floors;
- Blank wall length maximum length of 15’;
- Building lighting;
- And, mechanical and service area screening.

Planning Staff requests that the Planning Commission hold a work session to discuss the application and provide initial input, feedback, and direction to Planning Staff and the applicant so they can finalize their Design Review proposal and bring it back to the Planning Commission for a final decision. The feedback should direct the applicant and Planning Staff on whether the proposal generally complies with the standards for design modification of the D-2 zoning district found in chapter 21A.59 and attached to this Memorandum as Attachment A.

In addition to the Planning Commission discussing the 800 S + State Street project this work session is also an opportunity for the Planning Commission to give the property owner and applicant direction on the larger Sears Block. While the larger block plans are not part of the Design Review and the Planning Commission cannot make a determination, the Work Session allows the Planning Commission to give the property owner direction with what the Planning Commission would prefer to see on the future development of this city block.

## **BACKGROUND AND SITE CONTEXT**

The 800 S + State St project site is part of a larger block colloquially known as the Sears Block. The Sears Block includes the approximately  $\frac{3}{4}$  of city block 16 and is bordered by Main Street on the West, State Street to the East, and 700 and 800 South. The project site is proposed on the south-east corner of the Sears Block, and frontage on State Street and 800 South. Presently, this portion of the block is occupied by a vacant building, which was at one time occupied by Sears. The building is currently vacant and is nearly completely encompassed by a surface parking lot that spans the majority of the southern portion of the block. Prior to the Sears



*Image 1: 1911 Cooley Park, Seeing Salt Lake City*

commercial development, the south portion of the block was the site of the Cooley Ball Park and was surrounded by residential dwellings facing 800 South and State Street. This section of the Sears Block was first issued a building permit in 1945 for the construction of the Sears Roebuck Co. building, and another



permit was issued in 1966 for the current development configuration of the site; in 2018 Sears closed and the site remained vacant.



The 800 S + State project most significant boundary is State Street to the east. This section of State Street permits for a maximum vehicle speed of 30 MPH and allows for 3 lanes of travel in each direction with a median turning lane and on-street parking adjacent to the sidewalk. The sidewalk and east facing façade of the project will be impacted by the ongoing Life on State Plan, which is currently in development. The Life on State plan is proposing improvements that will facilitate a more pedestrian oriented streetscape and open this corridor to other forms of transport. These proposed improvements include a reduction in vehicle lane width, curb bulb-outs, an increase in pedestrian area or sidewalk, sidewalk amenities and signalized mid-block crossings. The applicant has been included in discussions with the Life on State team and has been working to ensure their plans and midblock roads meet the proposed sidewalk and midblock crossings planned along State Street.

Prior to the construction of the proposed building a subdivision plat will be required in order to relocate property lines and dedicate public right-of-ways along the north and west of the project site. A required subdivision process will be included as a condition of approval in Planning Staff's recommendation to the Planning Commission's final decision of the petition. Further details on the right-of-way dedication is included in the Larger Sears Block Discussion of this memorandum.

The applicant has worked with Planning Staff on improvements to the design of the building facades, particularly along the significant corridors of State Street and 800 South. These reiterations of the facades have brought the design and land use closer to the design standards in the D-2 zoning district and objectives in the Downtown Master Plan.

This project is the first of multiple phases of development on the Sears Block; it is also the first development with significant presence on State Street in the South State Street district of the Downtown Master Plan. This site will influence future development and redevelopment in this downtown district and will set the tone for the downtown support district along State Street – one of the most influential corridors in the city. The first redevelopment in the South State Street District is the 600 Lofts building fronting on 600 South and State. The 600 Lofts building is a smaller scale development than the subject property, with 6-stories, however, the façade length is greater at approximately 400’ in length. The majority of the existing buildings in this South State Street District are single or two-story and are generally at least 50 years old. All recent construction in the area is comprised of much larger scale than the older single and two-story structures. Additionally, the Downtown Master Plan supports larger and taller structures than what was historically built in this community. The designated zoning of the site, as a Downtown Support district, also supports larger scale development.



Figure 2: DOWNTOWN MASTER PLAN – SOUTH STATE STREET DISTRICT

### LARGER SEARS BLOCK DISCUSSION

The Majority of the Sears Block is represented by the applicants, Colmena group and Kimball Investments, and is primed for redevelopment. This is a unique opportunity in the City due to the potential size available for redevelopment, and is especially unique as a major commercial site so close to the Central Business District. The redevelopment of this larger block area will have significant impact on the future development along State Street and Main Street. This work session is an opportunity for the Planning Commission to provide the applicant feedback on their plans going forward before they submit for a Subdivision review; a Subdivision review is an administrative review and would not be required to be heard by the Planning Commission for a decision.

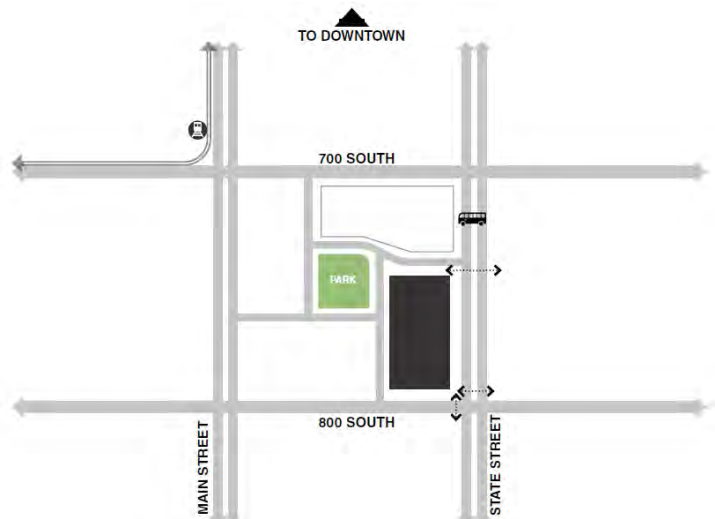


Figure 3: SEARS BLOCK PRELIMINARY PLANS

The southeast, southwest and northwest portions of the block are being considered for redevelopment by the applicant. The proposal for the block includes four new interior streets with an interior park in the center of the existing block. These interior streets would assist in breaking up the large 660’ width of block length and allow for the interior of the existing block to be better utilized. The interior streets will be approximately 66’ wide and will include, as part of the public right-of-way, a sidewalk and park strip. The landscaping and width of these elements will be required to meet City standards and will be reviewed prior to the recordation of the subdivision plat. The entire block is located in the Downtown Support (D-2) zoning district.



Another item to consider in the Sears Block proposal is the scale of the newly created lots and potential scale of the future proposed development on those lots. The proposed length and size of the newly recreated lots will direct the redevelopment of the block and will create a scale that is currently not typical of the area; the majority of the blocks along State Street were historically subdivided into smaller lots, the small size of the parcels dictates that new buildings will have a reduced street facing façade length with multiple buildings on the block face. Larger lots may not necessitate that structures are larger, but it opens development to that potential. Regardless of the size of the proposed blocks all new development on in the D-2 zoning district is required to meet all standards in the district and all Design Standards, any proposed modifications to the Design Standards will need to be reviewed by Planning Commission.

## DESIGN STANDARD REQUESTED MODIFICATIONS AND ISSUES FOR CONSIDERATION

The purpose of the Design Standards is to “utilize planning and architecture principles to shape and promote a walkable environment.., to foster place making as a community and economic development tool, protect property values, assist in maintaining the established character of the City, and implement the City’s master plans.” Modifications to the Design Standards are possible through the Design Review Process. Modifications of the Design Standards may be modified by Planning Commission if it is found that the modification

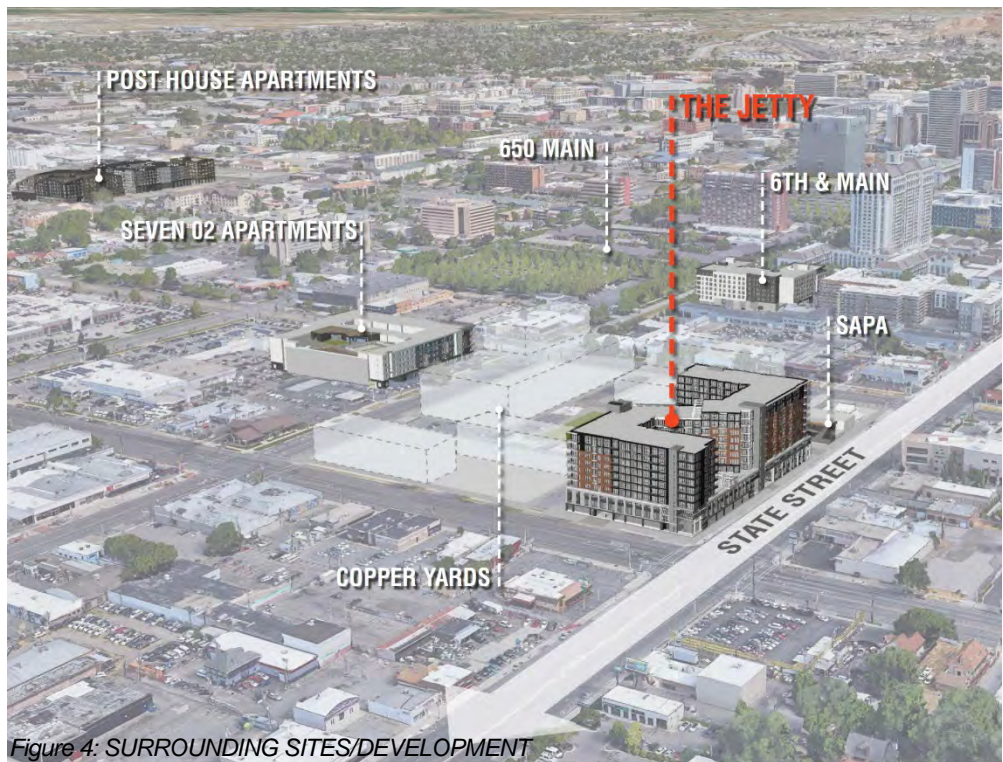


Figure 4: SURROUNDING SITES/DEVELOPMENT

meets the intent for the specific design standards required to be modified. The standards for Design Review are found in Attachment A of this Staff Report.

The design standards the applicant is requesting the Planning Commission to be modified from the underlying zoning district (D-2) includes:

- A modification to the allowed maximum 200’ length of street facing building façade;
- A modification to the required 65’ maximum building height in the D-2 district;
- And, a modification to the number of required building entrances.

### **Maximum Length of a Building Façade**

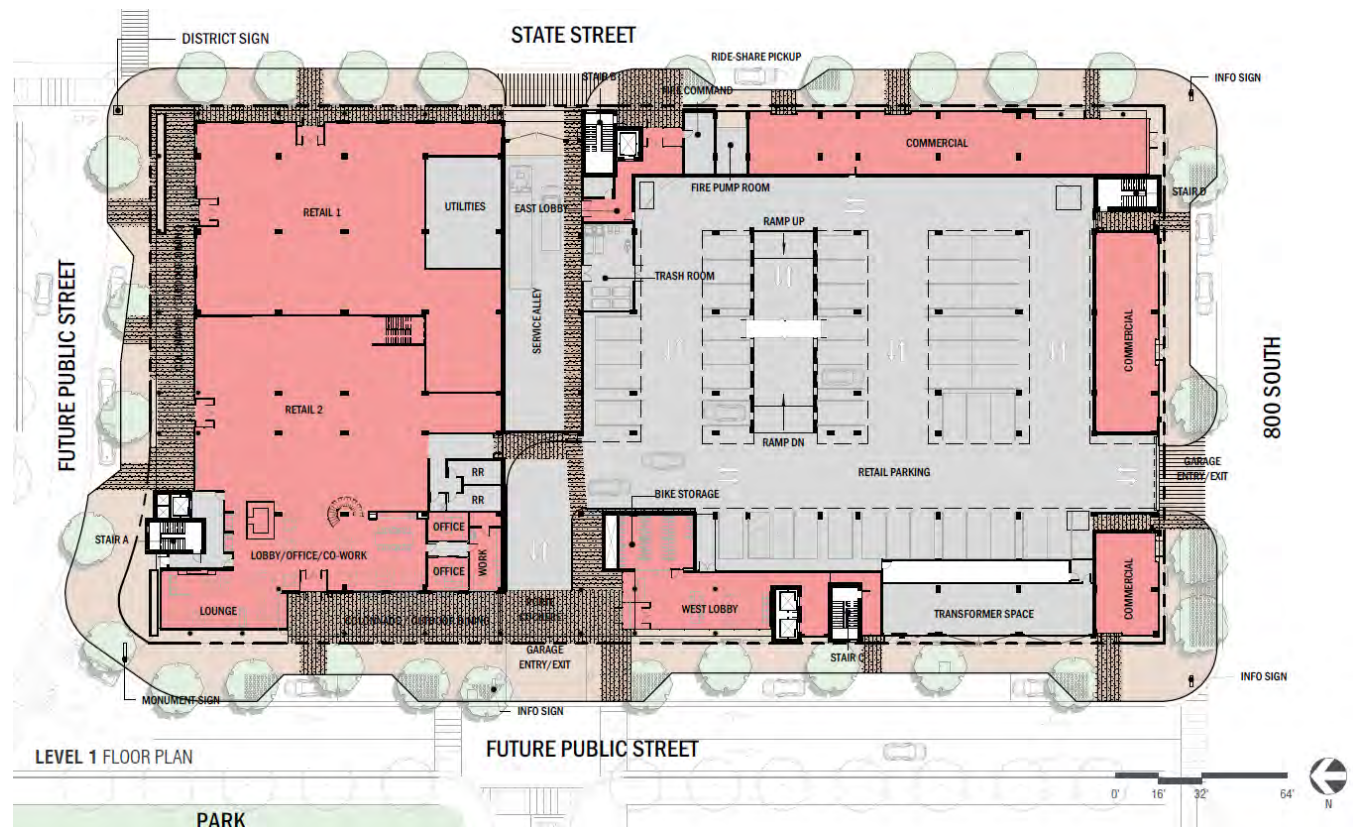
The maximum allowed length of a street facing façade in the D-2 zoning district is 200’. The 800 S + State building is proposed to have a total length of approximately 196’ on the north and south facades, which face

800 South and a proposed new street to the north, and will have approximately 373' along State Street to the East, and a proposed new street, Major Street, to the West. The east and west façades require a Design Review by the Planning Commission prior to approval as the façades exceed the maximum length.

The maximum building length standards were instituted spring 2019. These standards were a reaction to long building façade lengths that created excessive building lengths with minimal mitigation to the larger scale. The allowed modification to the Design Standards do not include a maximum façade length, rather the standards for Design Review, included as Attachment A, must be met prior to Planning Commission approval.

As previously explained, the proposed 800 S + State project is the first large scale redevelopment on the block and one of a few large-scale redevelopment projects in the area. The surrounding structures are a much smaller scale with buildings that are one or two-stories tall. However, newer structures proposed or are currently being built in the general vicinity are larger in scale with a generally height exceeding 3 or 4 stories. The proposed building, with the proposed size and scale, will have an impact on the future development along State Street and 800 South in the immediate area of the project site.

The 800 S + State Street project is proposed to be an 11-story structure with commercial and retail uses on the ground floor and second floor, on the upper floors 360 residential units with residential common areas and green spaces are proposed. The parking structure will be positioned on the interior of the structure on the first and second floors, on the third floor the parking lot will extend to the front façade but will be screened on all four sides of the building. The commercial and retail uses on the ground and first floors will be at least 25' in depth and each is proposed with an entrance along the street facing façade.





The majority of the retail square footage will be housed on the north portion of the structure, on the ground floor with a mezzanine and an open area that extends to the second floor. This north portion will also feature a colonnade and columns which provide a feeling of enclosure that is setback from the busy vehicular travel on State Street. The south portion of the structure's ground floor will also include commercial and retail space. This ground floor will be open to the second story above creating more open floor plans for the proposed retail space along State Street, 800 South, and the new mid-block street to the north.

### **Building Elements Addressing Excess Façade Length**

Stepping the upper floors back above the third floor. This step back on the open floors allows for the creation of open space for the residents of the building.

The stairwells located on State and Major streets display vertical building articulation and act as partitions between the two separate design features along the ground, second, and third floors.

A service alley is proposed to bisect the building, it is placed in the transition area of the northern and southern portions of the façade on State and Major Streets. and will allow service vehicles access only. This service alley will permit service vehicles only, limiting access onto State Street as egress only.

The ground and second floor of the structure will house the commercial uses proposed in the building.

On the north portion of the building, as delineated by the exterior stairwells and the service alley, the first two floors will be stepped back from the property line, allowing for the creation of a colonnade



Figure 6: SOUTH/EAST PERSPECTIVE

along the three facades of the building that face State, Major, and a proposed public street to the north. Along the colonnade and setbacks the proposal includes cement columns that extend from the third floor to the sidewalk.

The ratio of window-to-solid on the north portion of the building is greater than the south portion. The ratio of window-to-solid ratio spans the first and second floors, and far exceeds the glazing standards in the D-2 district.

The southern portion of the building is proposed to have a smaller window-to-solid ratio.

The veneer of the south portion of the building is proposed to include different building materials than the northern portion of the building. The differing material on the south portion will be light masonry, this masonry is proposed on the second and third floors, allowing for the screening of the parking lot on the third floor to be broken up as well.

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The southern portion of the building façade will have a 1' setback from the property line (as required by Public Utilities) instead of a 4' setback with cantilevered third floors, as the northern section of the building is proposed to have.

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These elements assist in the proposal meeting the standards for Design Review for exceeding the block length façade. These include changes in the vertical plane of the building (upper floors intermittent step backs) and material changes (the northern portion of the building facing Major Street and State Street will include different veneer materials than the southern portion of the building facing the same streets).



### ***Building Height***



Figure 8: SOUTH/EAST PERSPECTIVE



The underlying D-2 zoning district allows for an increase in building height from 65' to a maximum 120' with a Design Review and a determination by the Planning Commission. The 800 S + State project is proposing a new structure with a maximum height of 123'; this height includes a 5' parapet wall. Parapet walls are allowed to project 5' into the maximum building height; elevator and stairwell bulkheads are permitted to extend 16' into the maximum height. The building's top floor is proposed to be stepped with 11-stories on the northern



Figure 8: NORTH/WEST PERSPECTIVE

portion and 10-stories on the south. The fourth story and above is stepped back approximately 5' from the façade of the third floor; the fourth through the eleventh floor will also be intermittently stepped back, creating an S-shaped curve in the building. Upper floor step-backs are not a Design Standard required in the D-2 zoning district.

Because the request by the applicant is for a modification from the base district height standards only the design standards that are “directly applicable to the modification” may be considered when making a determination on the modification. The applicable design standards are listed in Section 21A.59.050 of the Salt Lake City Zoning Code.

**Standards for Building Height Design Review 21A.59.050**

**D. Large building masses shall be divided into heights and sizes that relate to human scale.**

1. Relate building scale and massing to the size and scale of existing and anticipated buildings, such as alignments with established cornice heights, building massing, step-backs and vertical emphasis.
  
2. Modulate the design of a larger building using a series of vertical or horizontal emphases to equate with the scale (heights and widths) of the buildings in the context and reduce the visual width or height.

3. Include secondary elements such as balconies, porches, vertical bays, belt courses, fenestration and window reveals.
4. Reflect the scale and solid-to-void ratio of windows and doors of the established character of the neighborhood or that which is desired in the master plan.

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**G. Building height shall be modified to relate to human scale and minimize negative impacts.**

*Human scale:*

1. Utilize step-backs to design a building that relate to the height and scale of adjacent and nearby buildings, or where identified, goals for future scale defined in adopted master plans.
2. For buildings more than three (3) stories or buildings with vertical mixed use, compose the design of a building with distinct base, middle and top sections to reduce the sense of apparent height.

*Negative impacts:*

1. Modulate taller buildings vertically and horizontally so that it steps up or down to its neighbors.
2. Minimize shadow impacts of building height on the public realm and semi-public spaces by varying building massing. Demonstrate impact from shadows due to building height for the portions of the building that are subject to the request for additional height.
3. Modify tall buildings to minimize wind impacts on public and private spaces, such as the inclusion of a wind break above the first level of the building.

*Cornices and rooflines:*

1. Cohesiveness: Shape and define rooflines to be cohesive with the building's overall form and composition.
  2. Complement Surrounding Buildings: Include roof forms that complement the rooflines of surrounding buildings.
  3. Green Roof And Roof Deck: Include a green roof and/or accessible roof deck to support a more visually compelling roof landscape and reduce solar gain, air pollution, and the amount of water entering the stormwater system.
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***Building Entrances***

The D-2 district requires building entrances at least every 50' along a street facing building façade. The purpose of this standard is to engage the public sphere and orient the building to the adjacent street. The proposed 800 S + State St project does not meet this standard. This standard is not met along the north, west, and east facades of the building, the separation between the building entrances along these facades include a range between 35'-90'.

It is Planning Staff's opinion that the intent of the Design Review Standards are being met as it relates to building entrances. This is due to the parking lot located in the interior of the building, the window to wall ratio



is greater than the underlying standards, and the proposal interacts with the sidewalk through seating areas and a colonnade. Planning Staff recommends allowing for a modification to the building entrance design standards

## **DISCUSSION**

This work session is an opportunity for the Planning Commission to review the 800 S + State project and the larger Sears Block designs and to guide Planning Staff and the applicant on the redevelopment of the Sears Block site. This work session is held prior to the request for a Design Review returning to Planning Commission for a final decision.

The immediate vicinity of the Sears Block and project site has not seen redevelopment at a scale this size since the initial development of the Sears Block. The development of this project size at this scale will have significant impact on the surrounding community and will influence how development occurs and how the downtown support district along State Street will be shaped.

Prior to approval it must be shown that the intent of the Design Review is met as well, the purpose of the Design Review is to, *“verify new developments are compatible with their surroundings, impacts to public infrastructure and public spaces are addressed, and that new development helps achieve development goals outlined in the adopted master plans of the City as identified in the purpose statements of each zoning district”*.

Overall it is Planning Staff’s opinion that the proposed height, massing, and number of commercial and residential units meets the intent of the Downtown Support district in that it, *“fosters the development of a sustainable urban neighborhood that accommodates commercial, office, residential and other uses that relate to and support the Central Business District”*. It is also Planning Staff’s opinion that the proposal also meets the goals of the Downtown Master Plan, in particular the South State Street Plan as it, *“utilizes interior blocks”, “maximize[s] visual transparency from [the] sidewalk into stores”, “Develop[s] a ‘garden model’ block development”, and “encourage[s] development of pedestrian-oriented businesses along State Street with ...residential above”*. However, the proposed height and street facing façade length will be a substantial change to this area as surrounding redevelopment in the area has been minimal. As the impact and intensity of the Commercial Business District continues to spread outward redevelopment of the surrounding block is likely as demand for retail and housing in this area is likely to continue.

### ***Questions for Planning Commission consideration to guide redevelopment:***

- Are the setbacks, material changes, and glazing sufficient to warrant a 85% increase in the street façade length while still maintaining a rhythm and repetition experience of the pedestrian?
- Does the architecture allow for mitigation of the visual impact of the additional height by providing a design with a distinct top, middle, and bottom interest in the structure?
- When determining whether the subject project relates to the height and scale of adjacent and nearby properties, should the consideration include the redevelopment potential of the adjacent properties?

# **ATTACHMENT A: STANDARDS FOR DESIGN REVIEW**

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Applications seeking modification of base zoning design standards, applicants shall demonstrate how the applicant's proposal complies with the standards for design review that are directly applicable to the design standard(s) that is proposed to be modified.

- A. Any new development shall comply with the intent of the purpose statement of the zoning district and specific design regulations found within the zoning district in which the project is located as well as the City's adopted "urban design element" and adopted master plan policies and design guidelines governing the specific area of the proposed development.
- B. Development shall be primarily oriented to the sidewalk, not an interior courtyard or parking lot.
  - 1. Primary entrances shall face the public sidewalk (secondary entrances can face a parking lot).
  - 2. Building(s) shall be sited close to the public sidewalk, following and responding to the desired development patterns of the neighborhood.
  - 3. Parking shall be located within, behind, or to the side of buildings.
- C. Building facades shall include detailing and glass in sufficient quantities to facilitate pedestrian interest and interaction.
  - 1. Locate active ground floor uses at or near the public sidewalk.
  - 2. Maximize transparency of ground floor facades.
  - 3. Use or reinterpret traditional storefront elements like sign bands, clerestory glazing, articulation, and architectural detail at window transitions.
  - 4. Locate outdoor dining patios, courtyards, plazas, habitable landscaped yards, and open spaces so that they have a direct visual connection to the street and outdoor spaces.
- D. Large building masses shall be divided into heights and sizes that relate to human scale.
  - 1. Relate building scale and massing to the size and scale of existing and anticipated buildings, such as alignments with established cornice heights, building massing, step-backs and vertical emphasis.
  - 2. Modulate the design of a larger building using a series of vertical or horizontal emphases to equate with the scale (heights and widths) of the buildings in the context and reduce the visual width or height.
  - 3. Include secondary elements such as balconies, porches, vertical bays, belt courses, fenestration and window reveals.
  - 4. Reflect the scale and solid-to-void ratio of windows and doors of the established character of the neighborhood or that which is desired in the master plan.
- E. Building facades that exceed a combined contiguous building length of two hundred feet (200') shall include:
  - 1. Changes in vertical plane (breaks in facade);
  - 2. Material changes; and
  - 3. Massing changes.
- F. If provided, privately-owned public spaces shall include at least three (3) of the six (6) following elements:
  - 1. Sitting space of at least one sitting space for each two hundred fifty (250) square feet shall be included in the plaza. Seating shall be a minimum of sixteen inches (16") in height and thirty inches (30") in width. Ledge benches shall have a minimum depth of thirty inches (30");
  - 2. A mixture of areas that provide seasonal shade;

3. Trees in proportion to the space at a minimum of one tree per eight hundred (800) square feet, at least two inch (2") caliper when planted;
  4. Water features or public art;
  5. Outdoor dining areas; and
  6. Other amenities not listed above that provide a public benefit.
- G. Building height shall be modified to relate to human scale and minimize negative impacts. In downtown and in the CSHBD Sugar House Business District, building height shall contribute to a distinctive City skyline.
1. Human scale:
    - a. Utilize setbacks to design a building that relate to the height and scale of adjacent and nearby buildings, or where identified, goals for future scale defined in adopted master plans.
    - b. For buildings more than three (3) stories or buildings with vertical mixed use, compose the design of a building with distinct base, middle and top sections to reduce the sense of apparent height.
  2. Negative impacts:
    - a. Modulate taller buildings vertically and horizontally so that it steps up or down to its neighbors.
    - b. Minimize shadow impacts of building height on the public realm and semi-public spaces by varying building massing. Demonstrate impact from shadows due to building height for the portions of the building that are subject to the request for additional height.
    - c. Modify tall buildings to minimize wind impacts on public and private spaces, such as the inclusion of a wind break above the first level of the building.
  3. Cornices and rooflines:
    - a. Cohesiveness: Shape and define rooflines to be cohesive with the building's overall form and composition.
    - b. Complement Surrounding Buildings: Include roof forms that complement the rooflines of surrounding buildings.
    - c. Green Roof And Roof Deck: Include a green roof and/or accessible roof deck to support a more visually compelling roof landscape and reduce solar gain, air pollution, and the amount of water entering the stormwater system.
- H. Parking and on site circulation shall be provided with an emphasis on making safe pedestrian connections to the sidewalk, transit facilities, or midblock walkway.
- I. Waste and recycling containers, mechanical equipment, storage areas, and loading docks shall be fully screened from public view and shall incorporate building materials and detailing compatible with the building being served. Service uses shall be set back from the front line of building or located within the structure. (See subsection [21A.37.050K](#) of this title.)
- J. Signage shall emphasize the pedestrian/mass transit orientation.
1. Define specific spaces for signage that are integral to building design, such as commercial sign bands framed by a material change, columns for blade signs, or other clearly articulated band on the face of the building.
  2. Coordinate signage locations with appropriate lighting, awnings, and other projections.
  3. Coordinate sign location with landscaping to avoid conflicts.
- K. Lighting shall support pedestrian comfort and safety, neighborhood image, and dark sky goals.
1. Provide street lights as indicated in the Salt Lake City Lighting Master Plan.

2. Outdoor lighting should be designed for low-level illumination and to minimize glare and light trespass onto adjacent properties and uplighting directly to the sky.
3. Coordinate lighting with architecture, signage, and pedestrian circulation to accentuate significant building features, improve sign legibility, and support pedestrian comfort and safety.

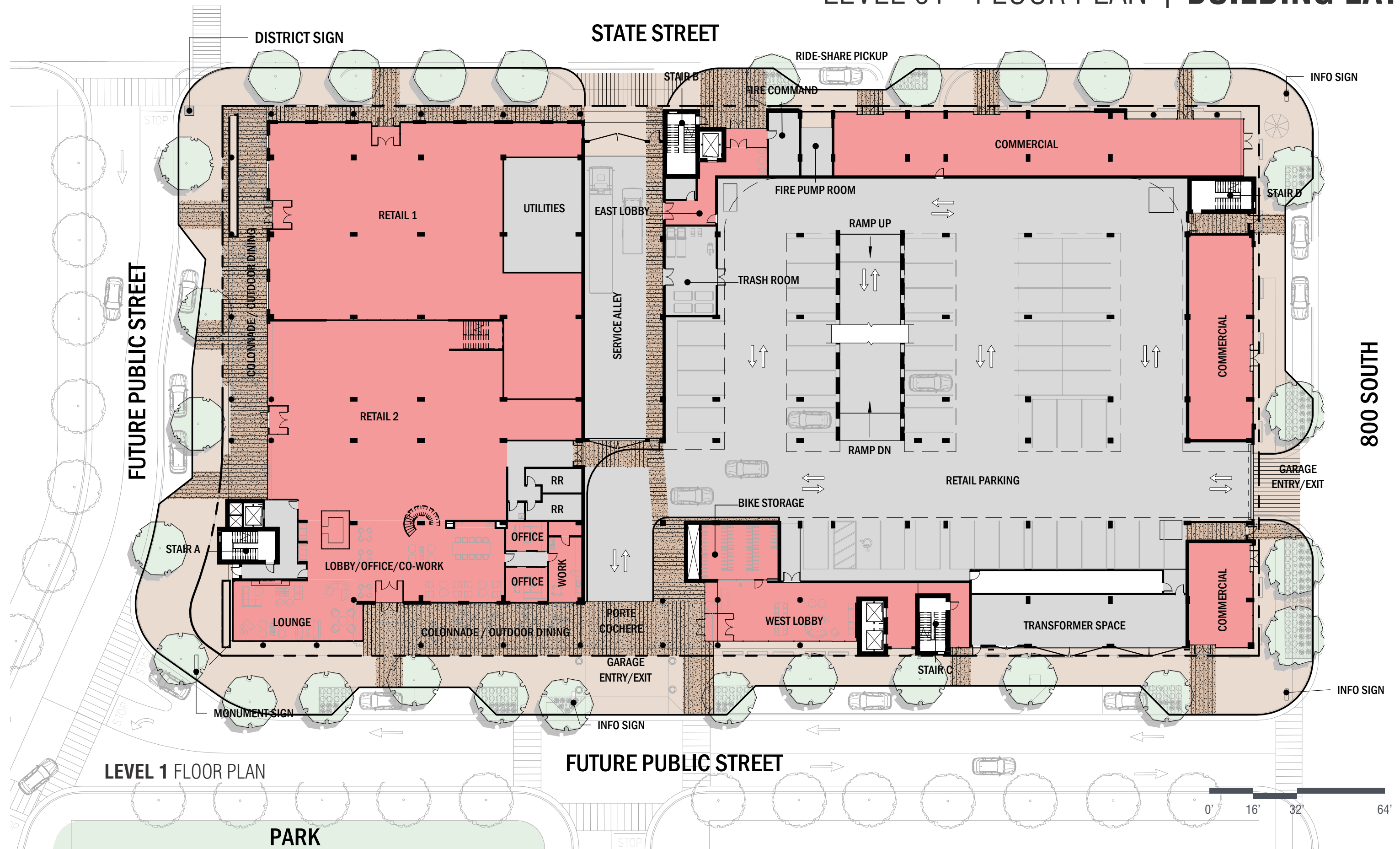
L. Streetscape improvements shall be provided as follows:

1. One street tree chosen from the street tree list consistent with the City's urban forestry guidelines and with the approval of the City's Urban Forester shall be placed for each thirty feet (30') of property frontage on a street. Existing street trees removed as the result of a development project shall be replaced by the developer with trees approved by the City's Urban Forester.
2. Hardscape (paving material) shall be utilized to differentiate privately-owned public spaces from public spaces. Hardscape for public sidewalks shall follow applicable design standards. Permitted materials for privately-owned public spaces shall meet the following standards:
  - a. Use materials that are durable (withstand wear, pressure, damage), require a minimum of maintenance, and are easily repairable or replaceable should damage or defacement occur.
  - b. Where practical, as in lower-traffic areas, use materials that allow rainwater to infiltrate into the ground and recharge the water table.
  - c. Limit contribution to urban heat island effect by limiting use of dark materials and incorporating materials with a high Solar-Reflective Index (SRI).
  - d. Utilize materials and designs that have an identifiable relationship to the character of the site, the neighborhood, or Salt Lake City.
  - e. Use materials (like textured ground surfaces) and features (like ramps and seating at key resting points) to support access and comfort for people of all abilities.
  - f. Asphalt shall be limited to vehicle drive aisles. (Ord. 14-19, 2019)

# **ATTACHMENT B: SITE PLAN**

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# LEVEL 01 - FLOOR PLAN | BUILDING LAYOUT



LEVEL 1 FLOOR PLAN

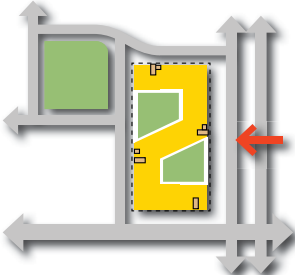
## **ATTACHMENT C: BUILDING ELEVATIONS**

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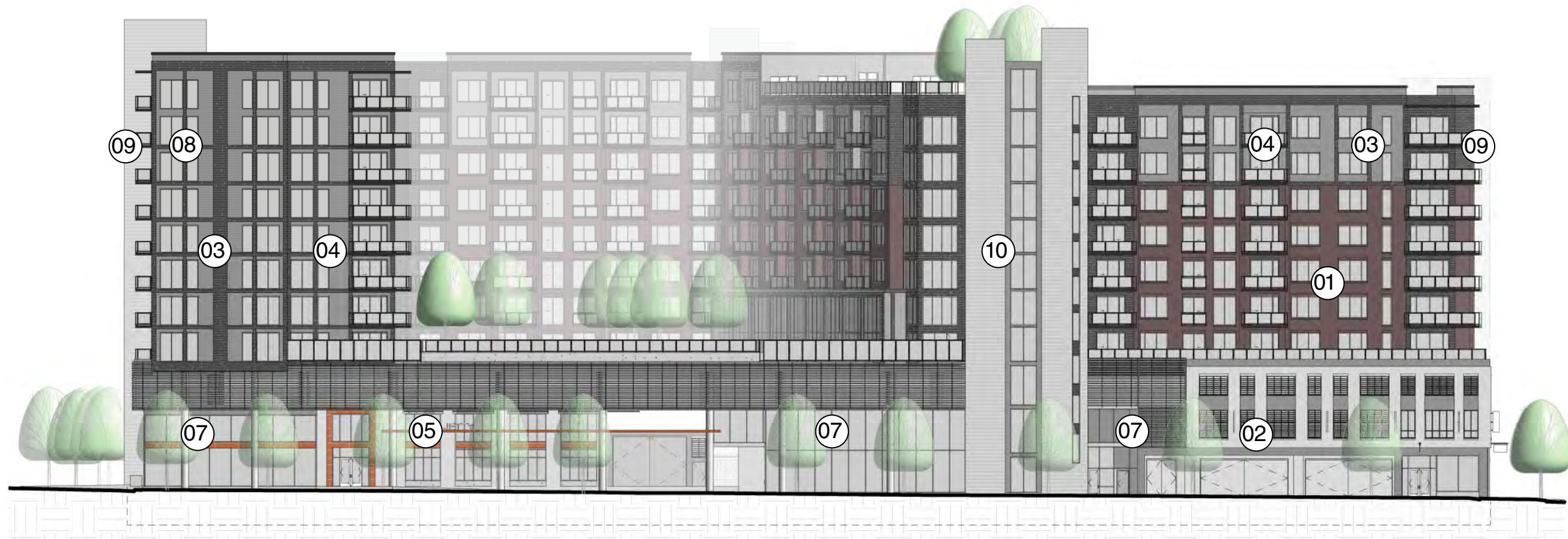
EAST ELEVATION (STATE STREET)



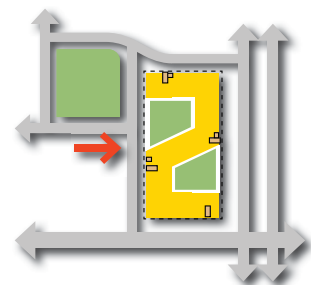
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|--------------------------|-----------------------------------|---|------------------------------------|
| 01 DARK MASONRY          | 04 LIGHT GRAY METAL PANEL         | 07 DARK ALUMINUM STOREFRONT                     | 10 FORM-LINER CONCRETE STAIR CORES |
| 02 LIGHT MASONRY         | 05 ACCENT COLOR METAL FASCIA      | 08 ALUMINUM WINDOW SYSTEM                       |                                    |
| 03 DARK GRAY METAL PANEL | 06 ACCENT COLOR STEEL TUBE SCREEN | 09 ALUMINUM HUNG BALCONIES AND JULIET BALCONIES |                                    |



# WEST ELEVATION | EXTERIOR DESIGN



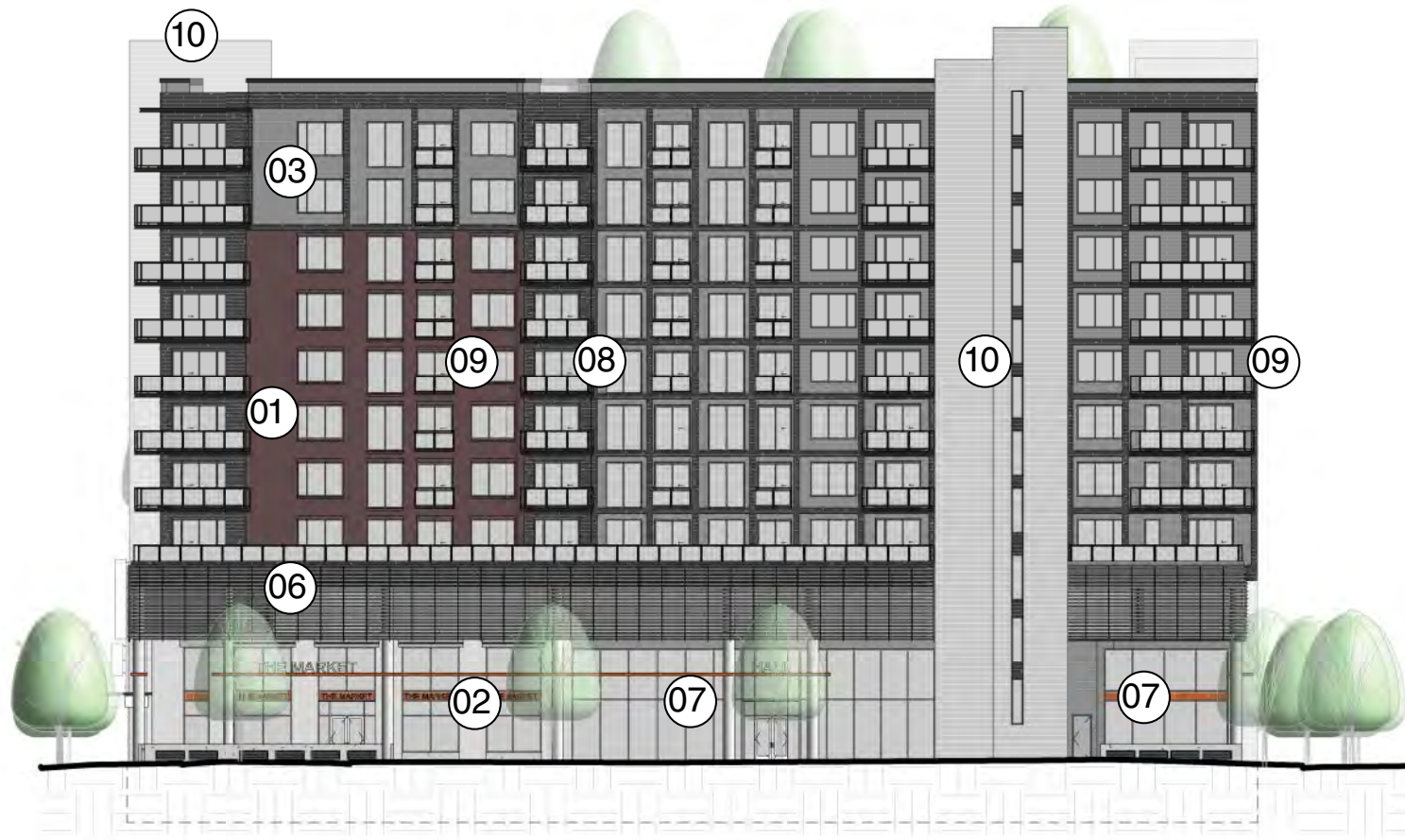
**WEST ELEVATION (MAJOR STREET)**



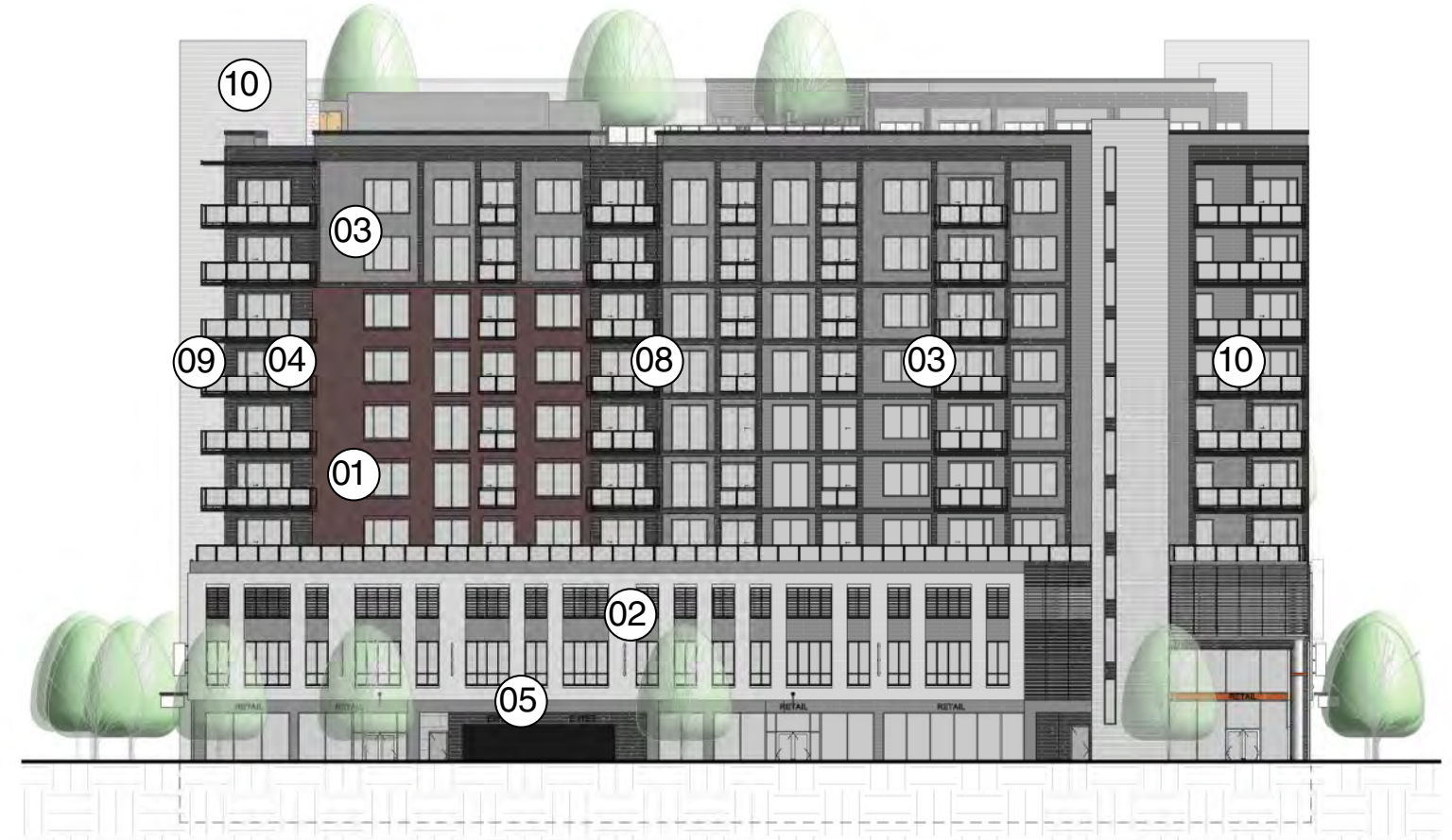
01) DARK MASONRY	04) LIGHT GRAY METAL PANEL	07) DARK ALUMINUM STOREFRONT	10) FORM-LINER CONCRETE STAIR CORES
02) LIGHT MASONRY	05) ACCENT COLOR METAL	08) ALUMINUM WINDOW SYSTEM	
03) DARK GRAY METAL PANEL	06) ACCENT COLOR STEEL TUBE SCREEN	09) ALUMINUM HUNG BALCONIES AND JULIET BALCONIES	



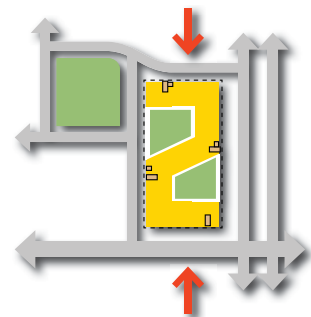
# NORTH & SOUTH ELEVATIONS | EXTERIOR DESIGN



**NORTH ELEVATION** (LOWELL STREET)



**SOUTH ELEVATION** (800 SOUTH)



01	DARK MASONRY	04	LIGHT GRAY METAL PANEL	07	DARK ALUMINUM STOREFRONT	10	FORM-LINER CONCRETE STAIR CORES
02	LIGHT MASONRY	05	ACCENT COLOR METAL FASCIA	08	ALUMINUM WINDOW SYSTEM	11	ROOFTOP MECHANICAL SCREEN
03	DARK GRAY METAL PANEL	06	ACCENT COLOR STEEL TUBE SCREEN	09	ALUMINUM HUNG BALCONIES AND JULIET BALCONIES		

## **ATTACHMENT D: SITE PHOTOS**

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*View of Site, North/West Perspective*



*View of Site, North/East Perspective from State Street*





*View of Site, South/West Perspective from 800 South*



*View of Site, South/East Perspective from 800 South and State Street*

## **ATTACHMENT E: APPLICANT'S SUBMITTAL**

---



# 800 SOUTH & STATE

SLC DESIGN REVIEW SUBMISSION | 2020.08.26





## 800 S & STATE

A beacon on the State Street corridor, the project at 800 S & State will serve as a catalyst for the blighted Sears block. After the Sears department store served the SLC community for over 50 years, their business model is no longer viable. What was once a sea of parking surrounding a dated windowless building will now be an activated destination residence to live, work/learn, and play within a pedestrian oriented community extending the urban city center further south.

At nearly 400,000 SF and 11 stories of mixed use, commercial (retail, food/beverage, co-work), parking and residential functions including Live-Work units on the ground floor, the project will capitalize on the views of surrounding mountains as well as the energy of the urban context. Art infused pedestrian links, activated by an energized ground level of refined retail hot spots of shops and boutiques at the edge of the street, are celebrated by porous residential, office and hotel lobbies (to come in future development phases) that define the people-centric essence of

the Salt Lake lifestyle. The building plinth will serve to activate the urban space of the neighborhood, while the residential common area amenities place compatible uses of community experience, comfort and community within the building.

The building is defined by bold and interesting building forms that utilize premium materials at points of shared enjoyment. Concentrating on the activation of the edges, the development is energized by a generous street scape with welcoming and engaging entryways. With a balance of light and form, in the spirit of the Rocky Mountains, the mixed uses at the ground level add convenience, support social encounters and echo the character of the community while offering opportunities for self-expression.

The project on the 1.74 acre site will support 360 high-end residential units above 15,000 SF of commercial/retail space with 400 on-site parking spaces, utilizing both above and below grade levels. A retail base and live-work units will flank the street facing facades of the

building in order to conceal the parking at grade. The residential units will be composed of Studio, 1 bedroom, and 2 bedroom units in square footages ranging from 550 SF to 1825 SF, resulting in an overall density of 207 units per acre.

The Project will be constructed with the IBC classification of "Type IB Construction" and will be built concurrently with the overall development of other portions of the block, which includes construction of public streets, utilities, and stormwater detention. Work on the block beyond the limits of the Project as shown on the drawings will be completed in future phases.

# TEAM

**OWNER**  
COLMENA GROUP



**OWNER**  
KIMBALL INVESTMENT CO



**ARCHITECT**  
OZ ARCHITECTURE



**CIVIL**  
CIR ENGINEERING



**STRUCTURAL**  
KL&A



**MEP**  
BG BUILDINGWORKS



**LANDSCAPE**  
ARCSITIO



**MANAGEMENT**  
GREYSTAR





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- SOUTH STATE AXON
- SOUTH STATE PLAN
- EXISTING SEARS BLOCK
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- OBJECTIVES AND STANDARDS
- A NOTE FROM DEVELOPMENT

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- DECONSTRUCTION STRATEGY
- EXISTING & PROPOSED PARCELS

## PARCEL 3

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- PERSPECTIVES
- MATERIAL PALETTE
- ELEVATIONS

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- PEDESTRIAN VIEWS
- PLINTH LEVEL VIEWS

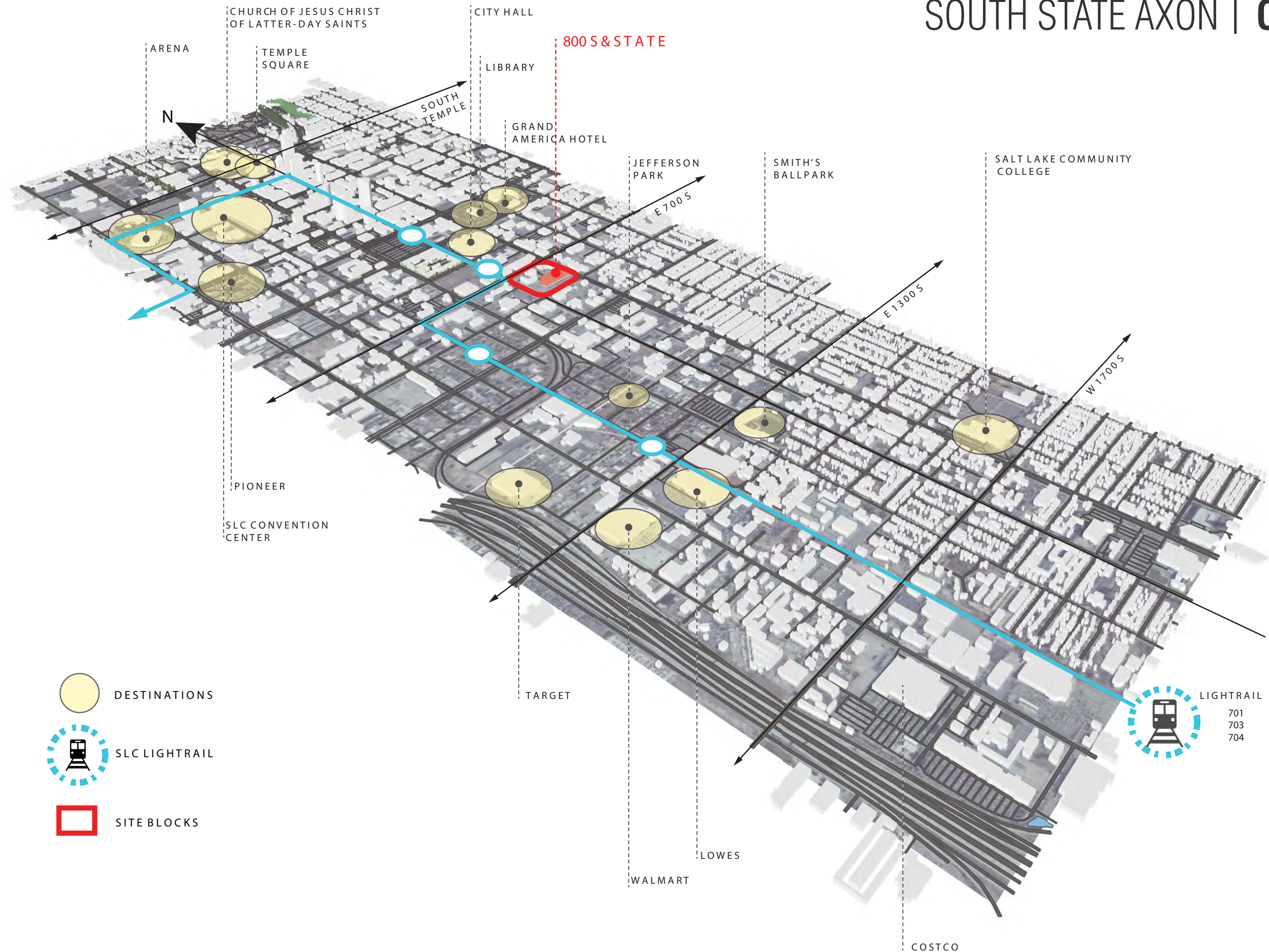
## CODE & ZONING

- D-2 GUIDELINES
- COMPLIANCE DIAGRAMS
- DESIGN STANDARDS

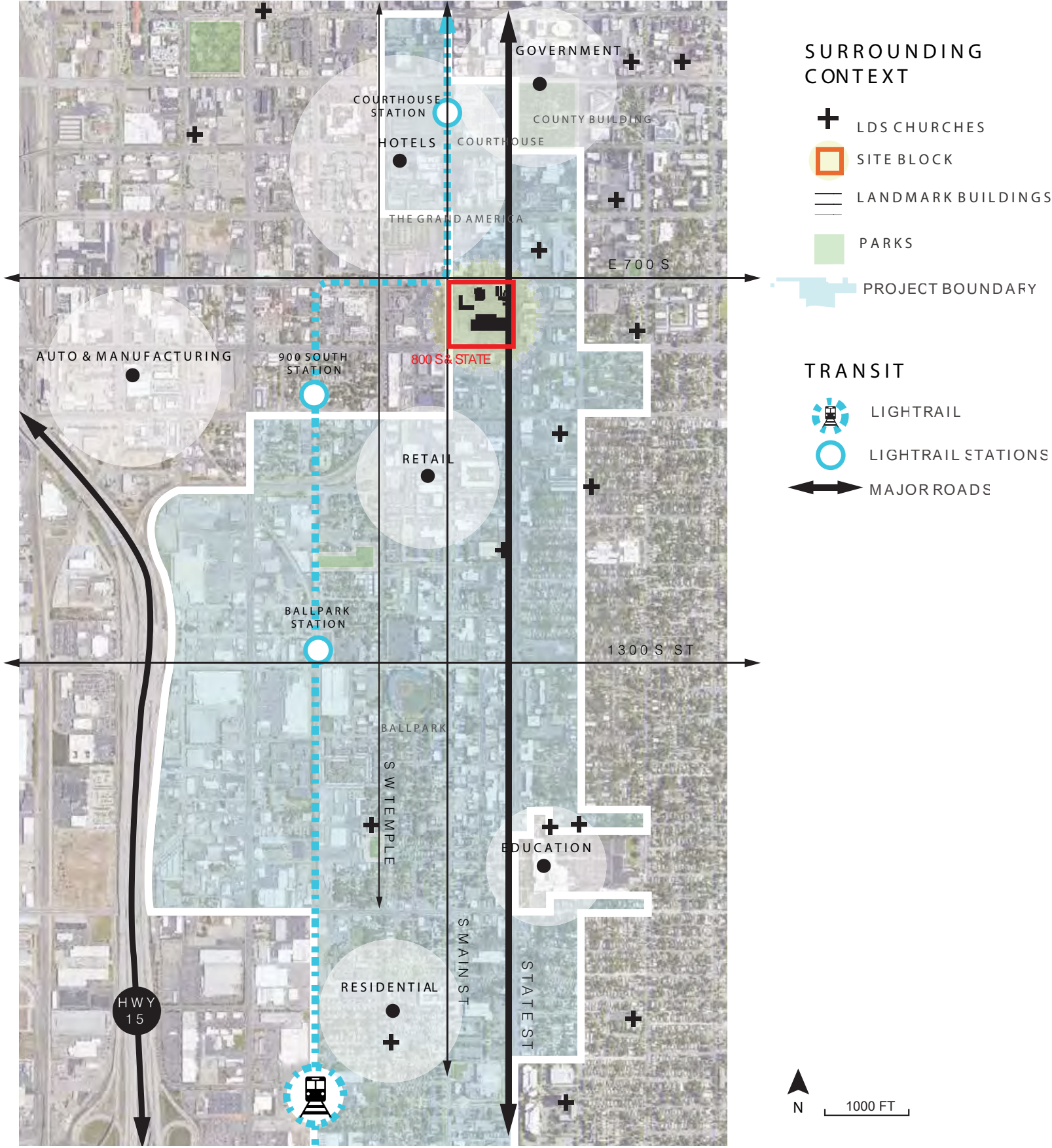
## SITE DESIGN

- CIVIL NARRATIVE
- PARCEL ADJUSTMENT
- SITE PLAN
- GRADING & DRAINAGE PLAN
- UTILITY PLAN

# SOUTH STATE AXON | CONTEXT







**The Big Picture**

The State Street Project Area covers a large expanse of Salt Lake City, and comprises a range of districts and neighborhoods with differing land uses, urban design characteristics, and transportation features. A key component of the State Street project area is State Street itself (United States Highway 89), from 400 South to 2100 South. State Street was once the primary north/south highway connecting Salt Lake City with adjacent cities prior to the construction of Interstate-80 (I-80) and Interstate-15 (I-15), and it continues to be a critical gateway to Salt Lake City.

**Current Conditions**

Some community members believe that State Street's auto-centric focus, its excess commercial capacity, and its auto-oriented infrastructure have given rise to crime, vacant buildings, and an abundance of underutilized urban land in more recent decades. Many also believe that conditions on State Street have negatively impacted economic development and public safety in the residential neighborhoods throughout the State Street Project

Area. The city blocks within the immediate neighborhood are poised for redevelopment as visionaries begin to address the current and future needs of downtown. The development of the Patrinely office buildings, a block away, the addition of a TRAX station along Main Street, and the first phase of this much larger project, are planting the seeds for the reactivation of the city streets.

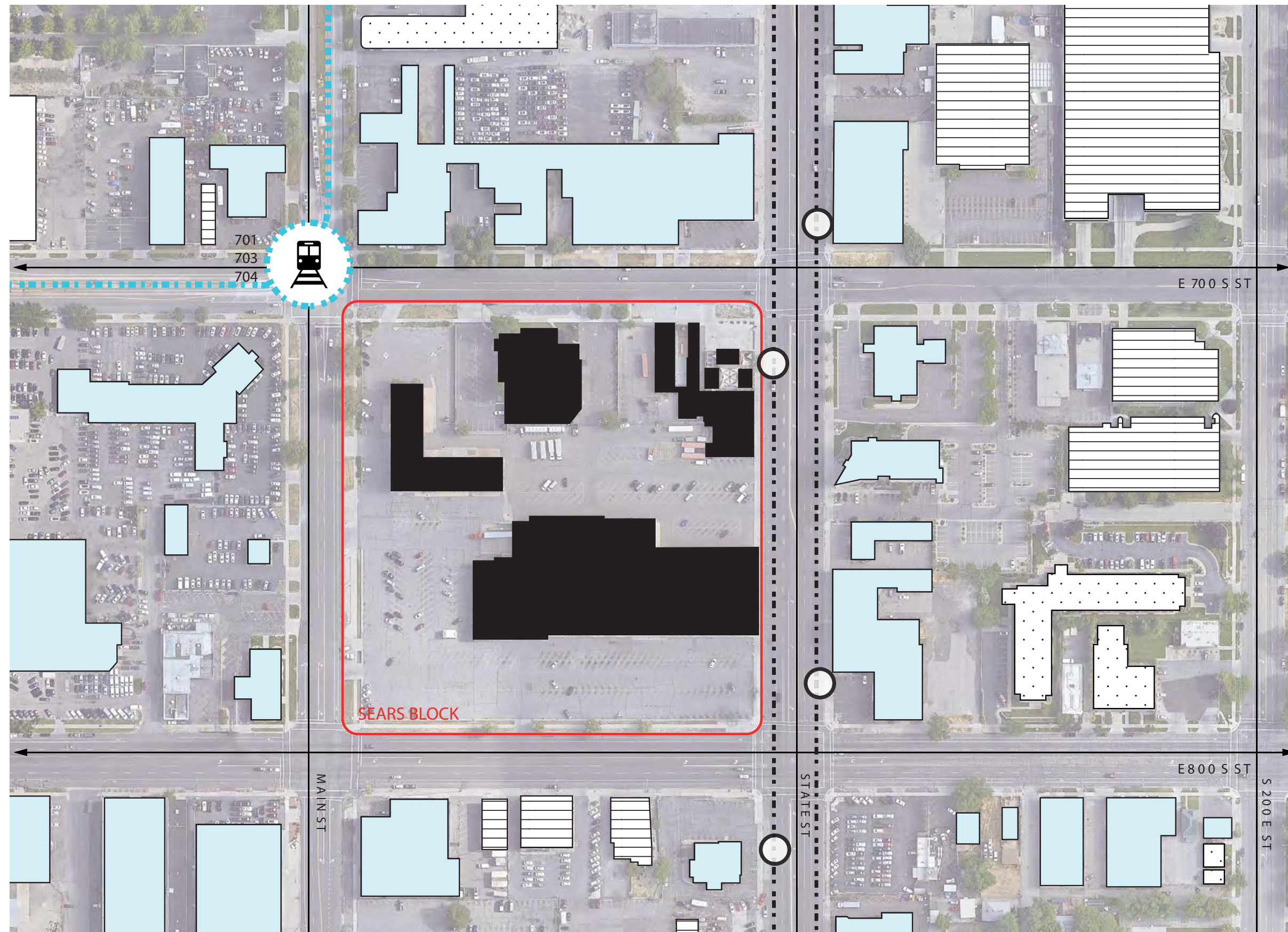
**The City Vision**

State Street has long been an automobile-focused corridor, and was a commercial and social hub for the Salt Lake Valley in the 1950's and 1960's. In more recent years, there has been increased interest from the community and City and regional planners to revitalize State Street in a way that respects the corridor's auto-oriented focus while expanding its uses to include more pedestrian-friendly and livable features. The overarching vision of the State Street Project Area is to promote a livable urban community with a strong urban design identity that preserves and enhances the integrity of its existing residential neighborhoods.






Source: SLCRDA - CRA Plan






# EXISTING SEARS BLOCK | CONTEXT



## SURROUNDING CONTEXT

-  CIVIC CENTERS
-  RETAIL & FOOD
-  EXISTING BUILDINGS
-  HOUSING
-  VACANT

## TRANSIT

-  SLC LIGHTRAIL
-  200 BUS ROUTE
-  200 BUS STOPS





EXISTING BUILDING | **CONTEXT**



**NORTHWEST VIEW**



**NORTHEAST VIEW**



**SOUTHWEST VIEW**



**SOUTHEAST VIEW**





# SURROUNDING BUILDINGS | CONTEXT



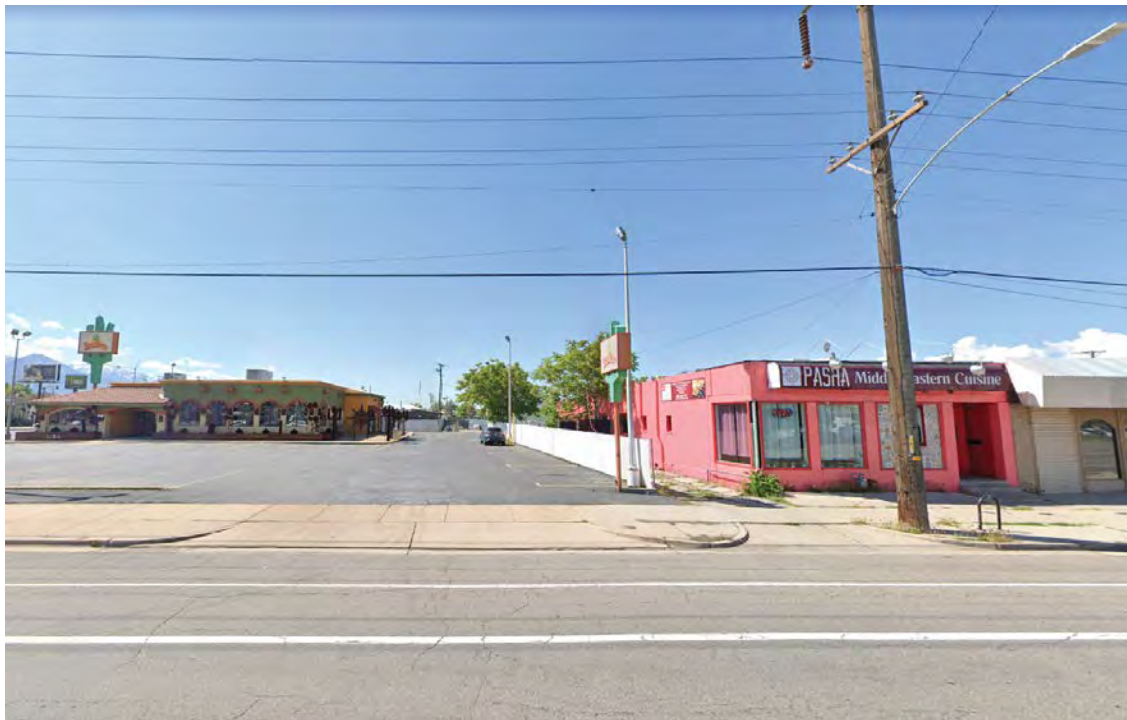
**E 700 LOOKING SOUTH**



**STATE LOOKING WEST**



**STATE LOOKING EAST**



**E 800 LOOKING SOUTH**



**STATE LOOKING EAST**





# OBJECTIVES

**Neighborhood Revitalization**



**Commercial Corridors**



**Employment Centers**



**Housing**



**Public Spaces & Transportation**

# GUIDING STANDARDS

- Encourage transit-oriented development
- Encourage a pedestrian-oriented, walkable environment with connections
- Encourage residential and commercial development on and near the State Street Corridor
- Encourage place-making and a vibrant destination area
- Encourage and promote sustainable practices
- Encourage the highest aesthetic standards possible - providing the greatest possible public value
- Support population growth and stability
- Coordinate with other City Departments and Divisions
  - Work with Salt Lake City Planning Division to review existing zoning codes and potentially make enhancements to zoning and allowable land use to promote and allow development to evolve with the changing market

## PROJECT VISION

The vision of this development is to utilize the Sears Block as a catalyst for growth in this part of the city. This area of downtown Salt Lake City is a gritty, authentic, urban hub that has a strong focus on food and entertainment. It is located south of the CBD, to the east of the Granary, West of the Maven District/9th and 9th and north of the Ballpark neighborhood. This site is a true hub that will connect each of these other neighborhoods.

One of our main goals is to break down the super block (original 10 acre block) by reintroducing an interior street grid. This creates a more pedestrian friendly atmosphere and helps activate the center of the block. Given our location in this neighborhood, we want to supplement the uses that already exist, while also providing some pieces that are missing. We are true believers of the City's Life on State plan, a key component being the "Life" part. In addition to street activation, it is important to bring residential units to this part of State Street to help support the adjacent retail as well as further the activation of the street. Another key component of the project is to add much needed green space into this urban environment. Our central lawn will serve as a backyard

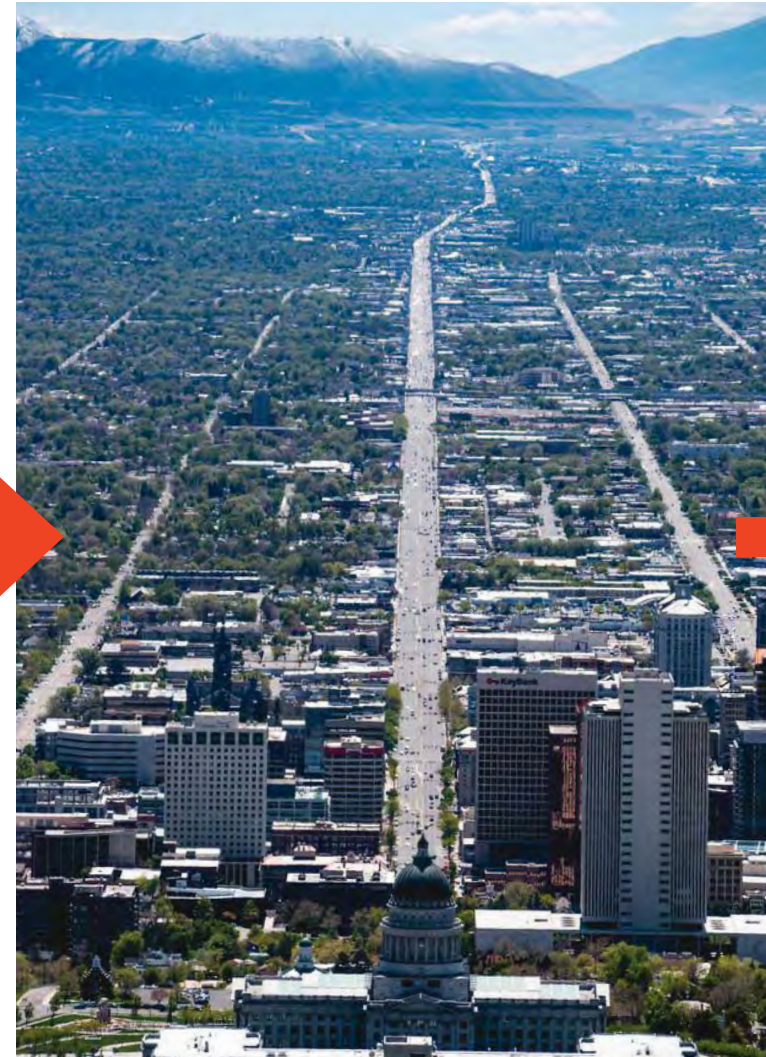
for not only our residents and tenants, but also for everyone in the City to enjoy. This lawn will be the heart of the development that brings people together for dining, recreation and relaxation. The vision for our commercial space is to create vibrant, mixed-use spaces that blur the lines between tenants and uses. We envision an urban grocer that shares space with local food vendors who operate a food hall that also coexist with a leasing office and coworking space. This will be a destination that draws the entire community together.

We view this project as the continuation of what is already great in this area. This is a space where nature, art and culture converge to form a wholly new idea – an idea that is fertile ground for authentic connectivity, creativity and thriving together. We are reimagining a vibrant, inclusive new community where everyone is welcome and can discover and embrace life fully.





**PLAT OF ZION AGRICULTURAL BLOCK**



**MODERN DAY SLC 132' WIDE STREETS**



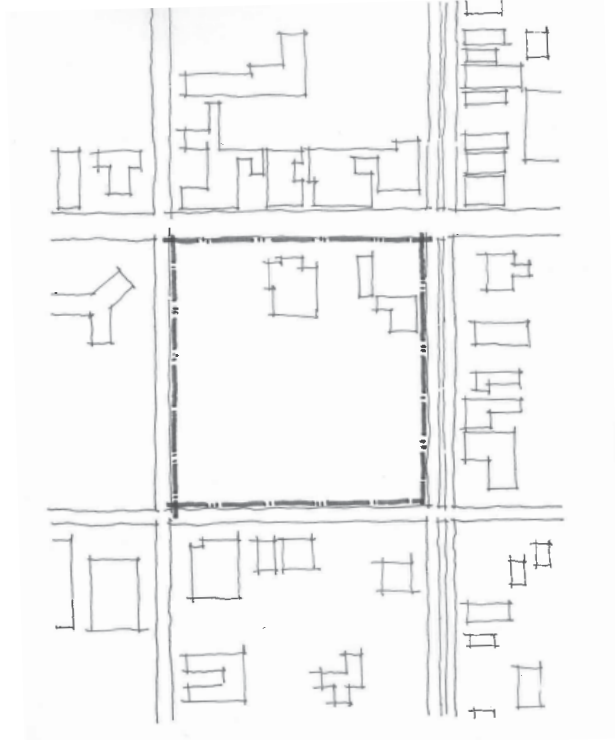
**SEARS BLOCK 660' LENGTH**



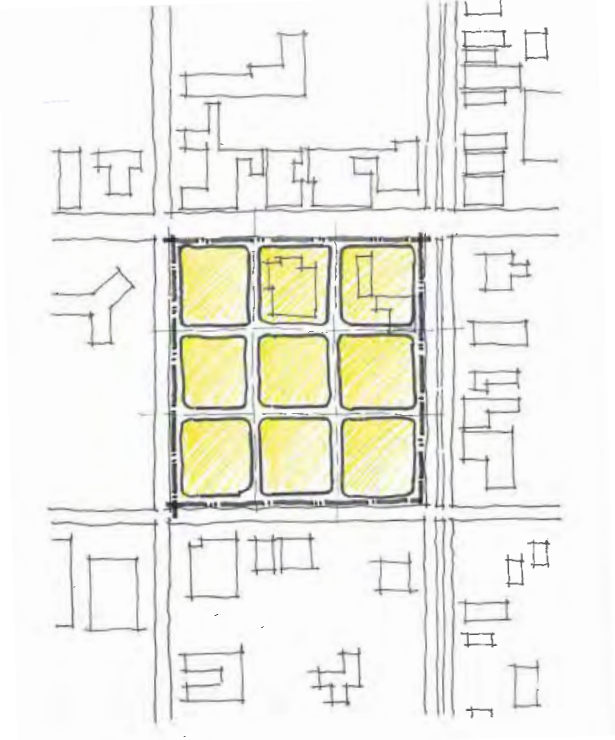
### The Block

As a gateway to this new complex metropolitan community, the proposed development denotes the idea of a 'doorway' within the heritage of memorable clarity that was espoused by the Plat of Zion. This concept echoes the primary role of the project to create a strong yet welcoming edge, with defined and accessible passageways into the Central Park core for both vehicles and pedestrians.

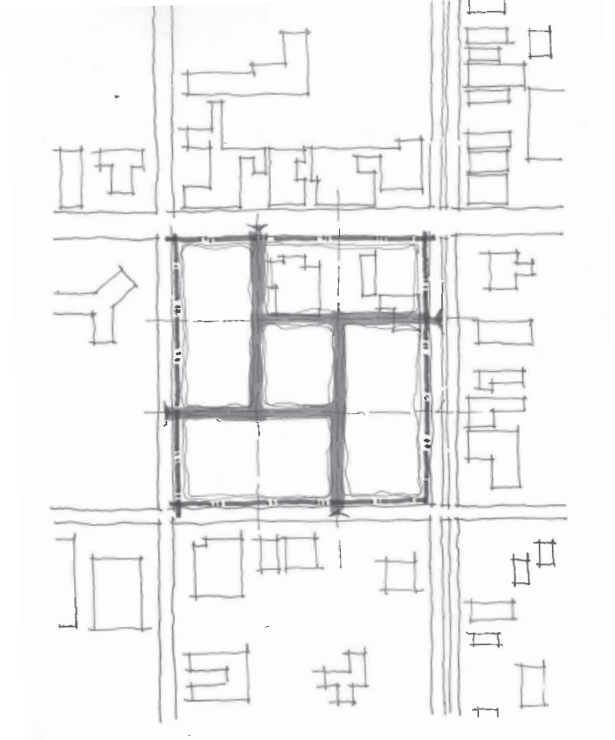
The 660' long super block was broken into nine quadrants approximately 200'x200' separated by street ROW. We further refined the nine quadrants by connecting the ones along the edge to optimize the number of vehicular penetrations to the core. This strategy optimizes the circulation of vehicles within the site, as well as allowing pedestrian connectivity through the plats to the inner park.



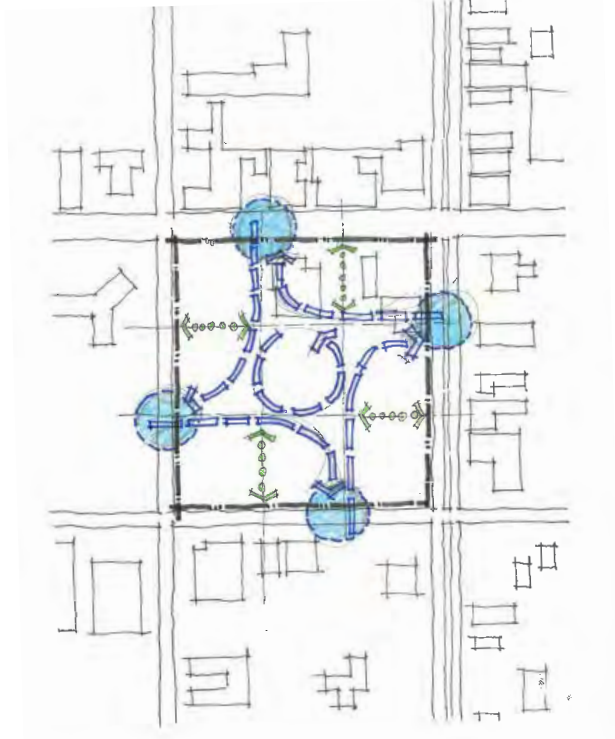
01 BLOCK



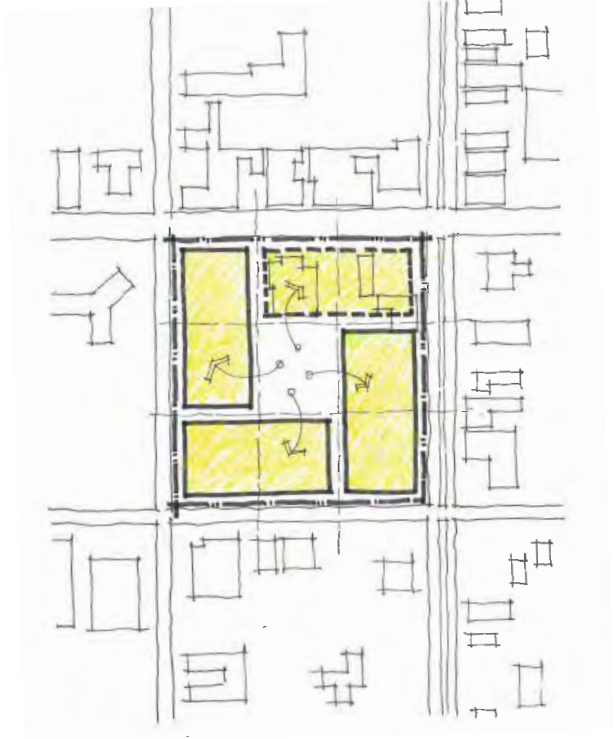
02 SUBDIVISION



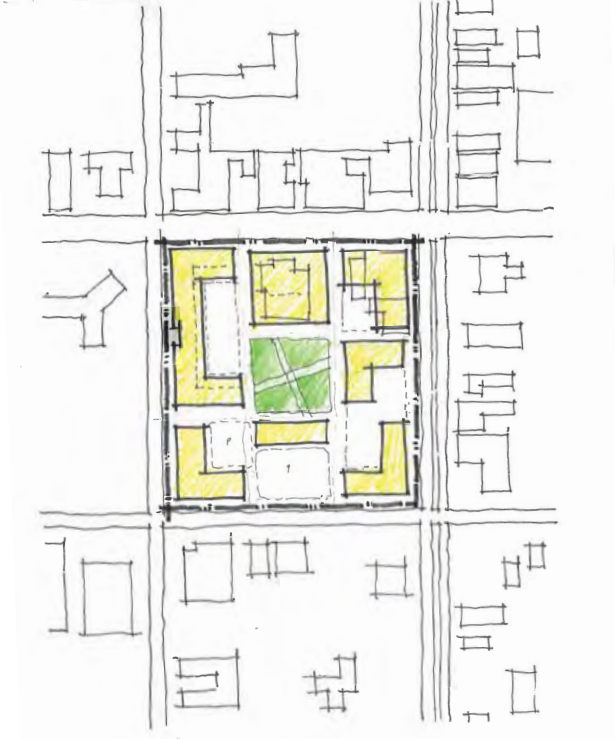
03 CIRCULATION



04 NODES



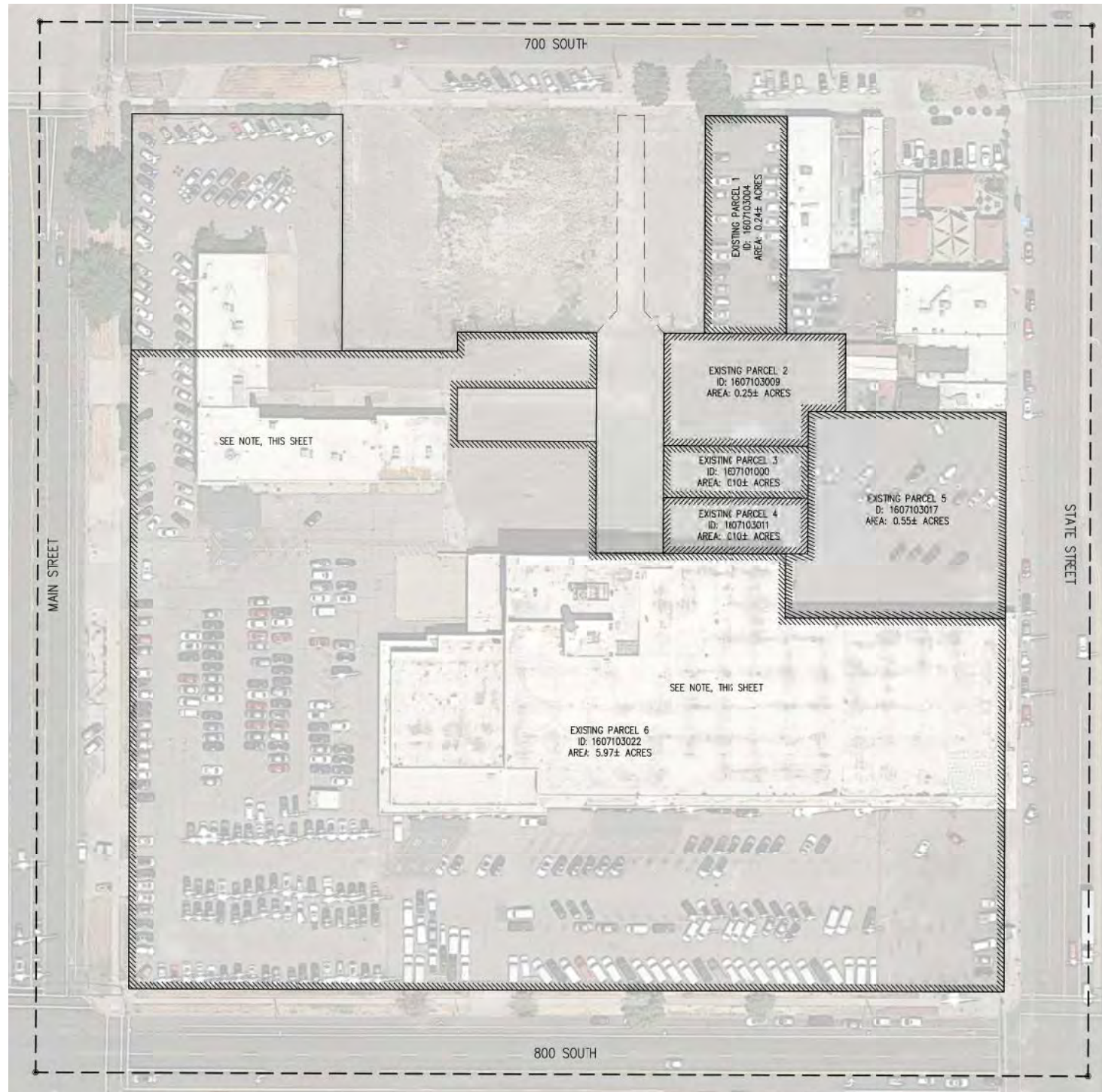
05 CONSOLIDATION



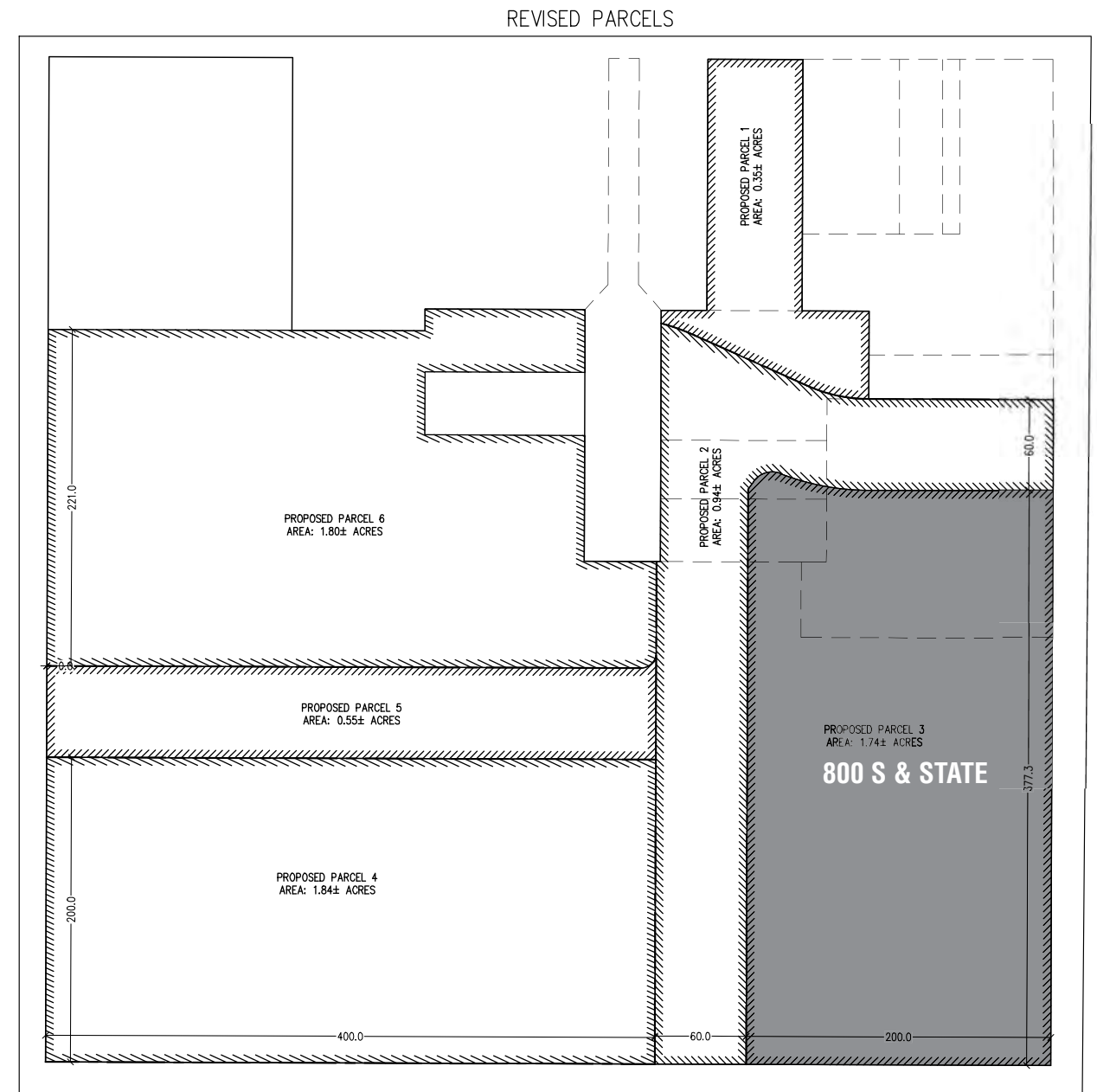
08 BUILDING FOOTPRINT



# EXISTING & PROPOSED PARCELS | SEARS BLOCK

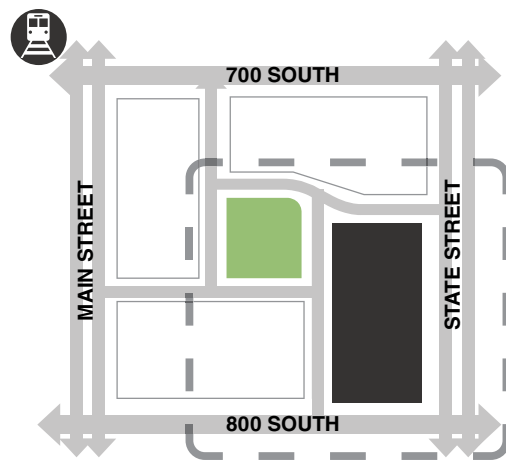


**EXISTING PARCELS**



**PROPOSED PARCELS**



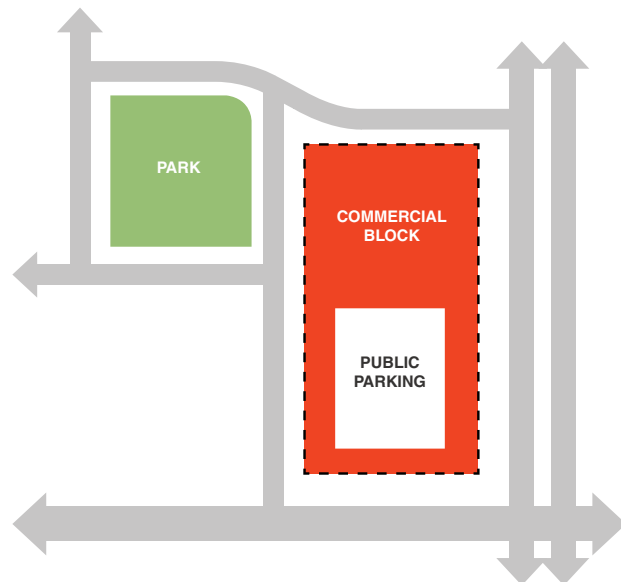


KEY PLAN

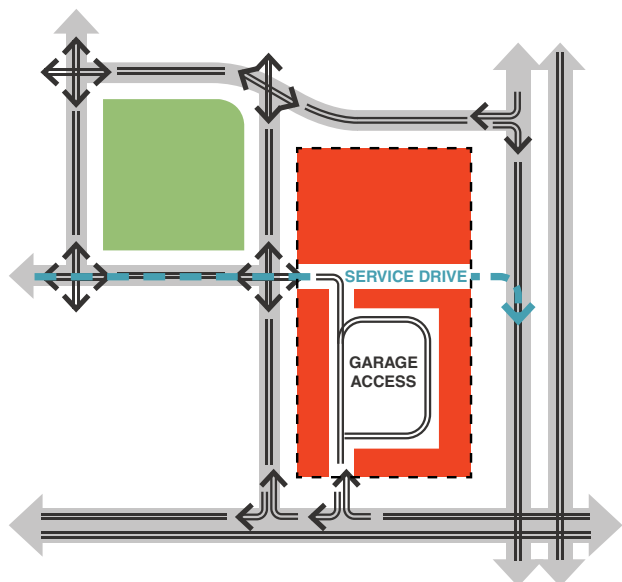
- GREEN SPACE
- COMMERCIAL
- RESIDENTIAL

# USE DIAGRAMS | PARCEL 3

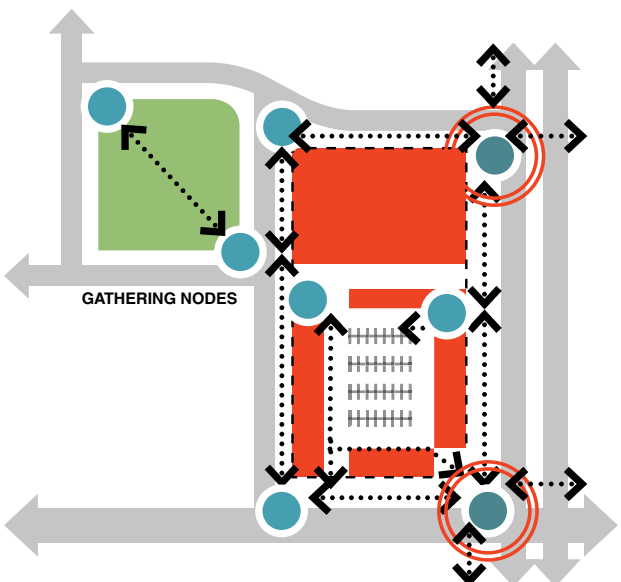
GROUND FLOOR CONCEPT



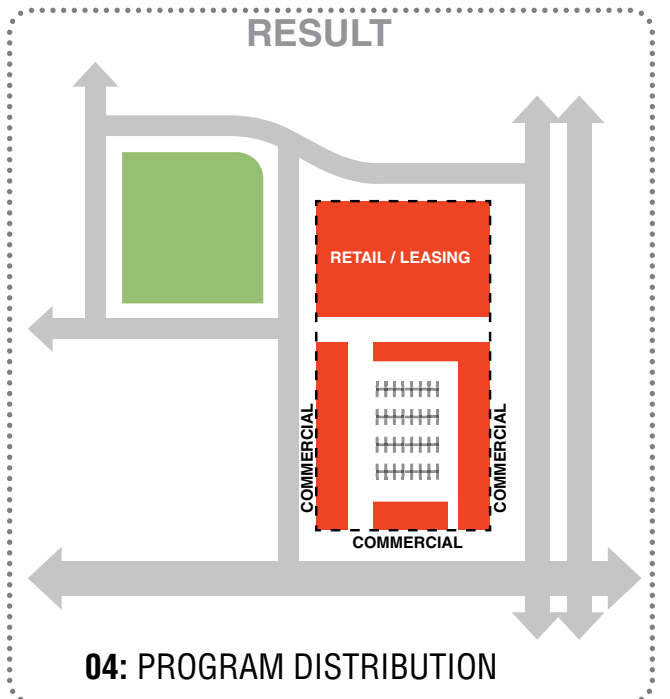
01: COMMERCIAL BLOCK



02: VEHICULAR CIRCULATION

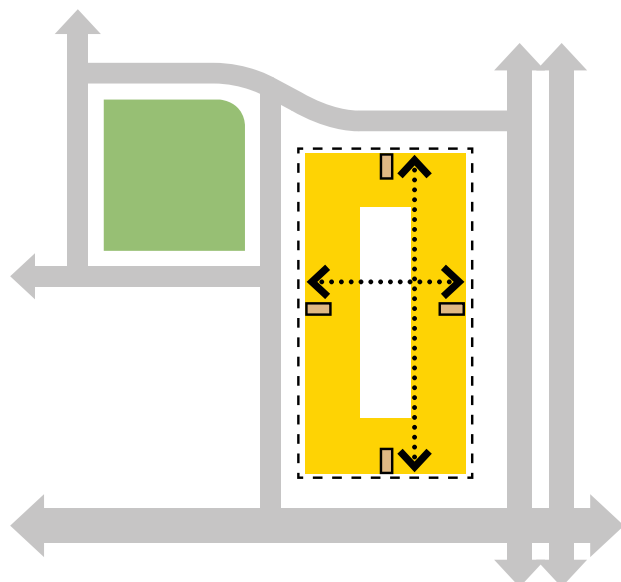


03: PEDESTRIAN CIRCULATION

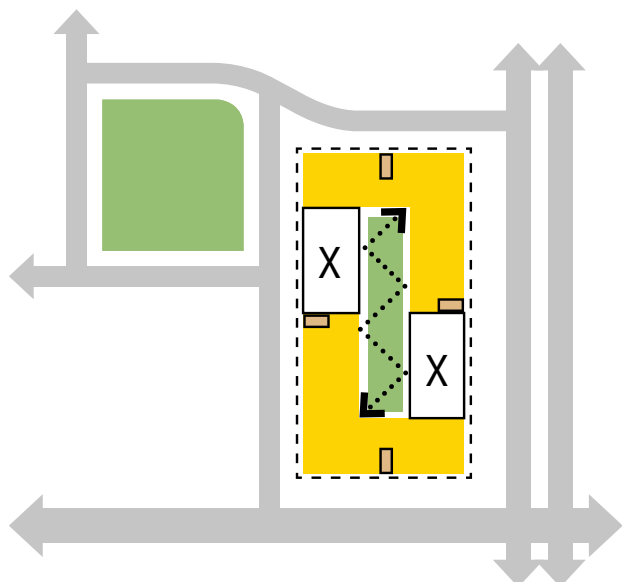


04: PROGRAM DISTRIBUTION

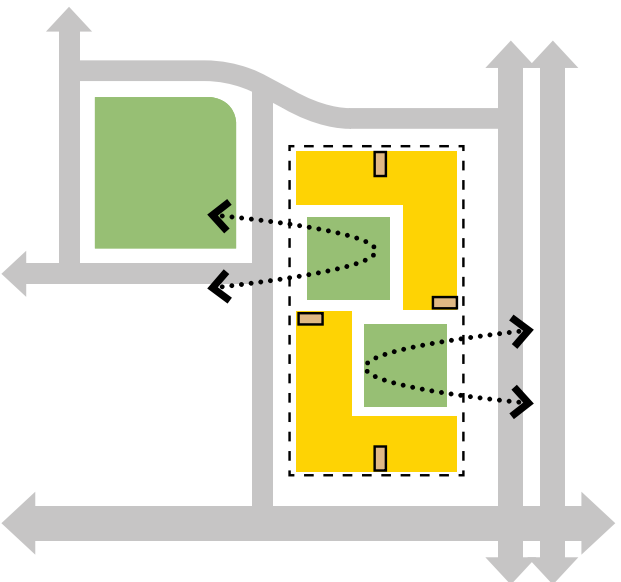
UPPER LEVEL CONCEPT



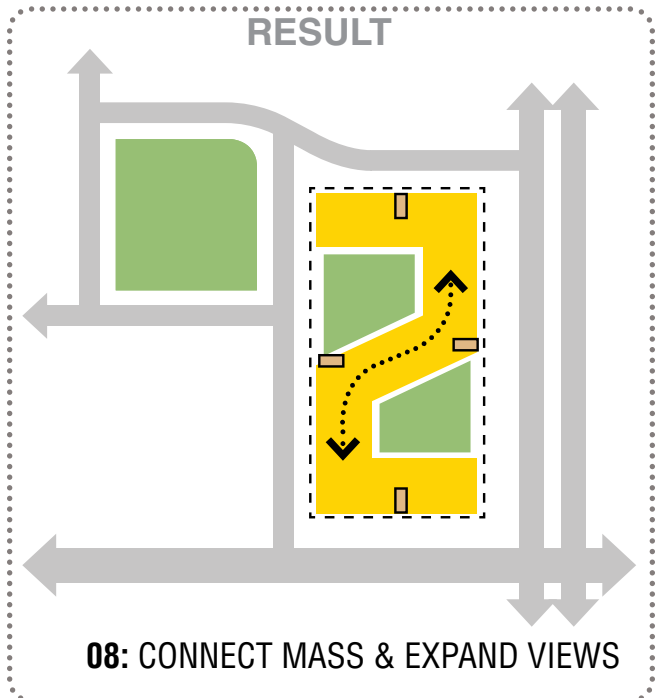
05: MAXIMIZE PLINTH & DISPERSE CORES



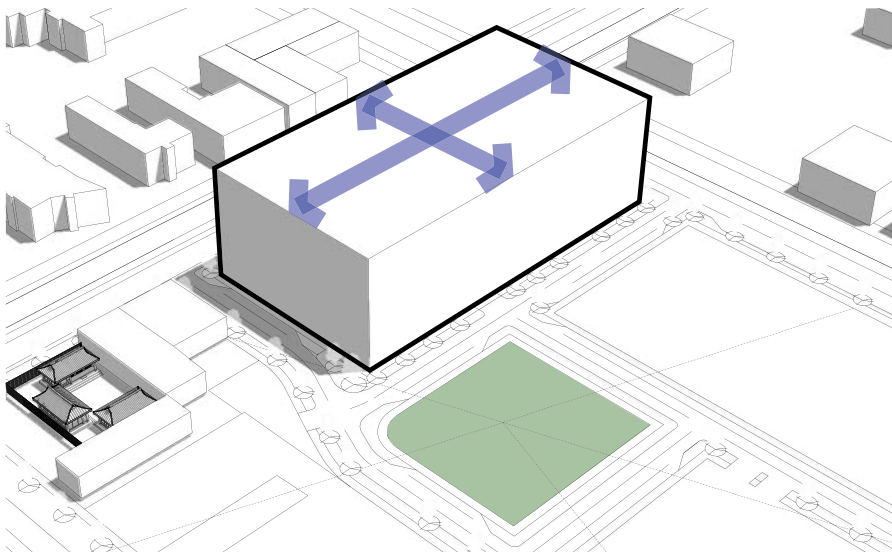
06: OPEN CONFINED COURTYARD



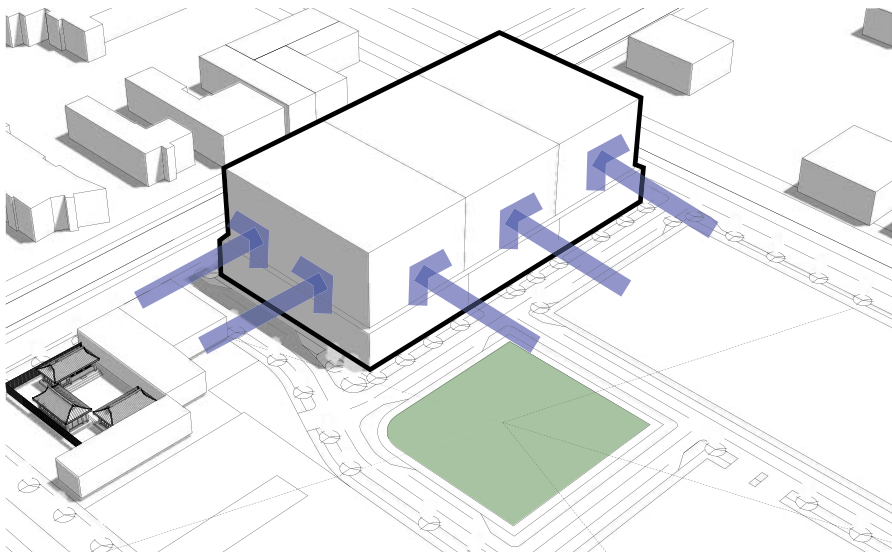
07: DEFINE VIEWS & AMENITY DECKS



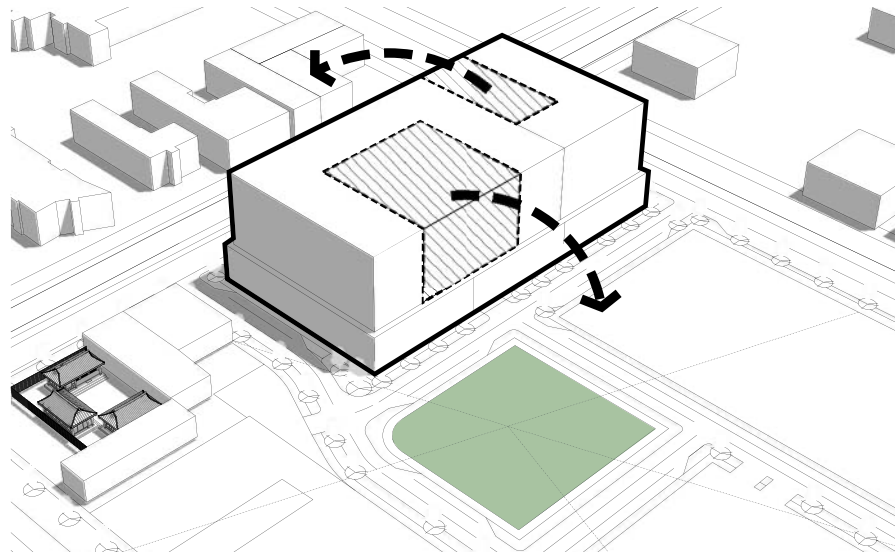
08: CONNECT MASS & EXPAND VIEWS



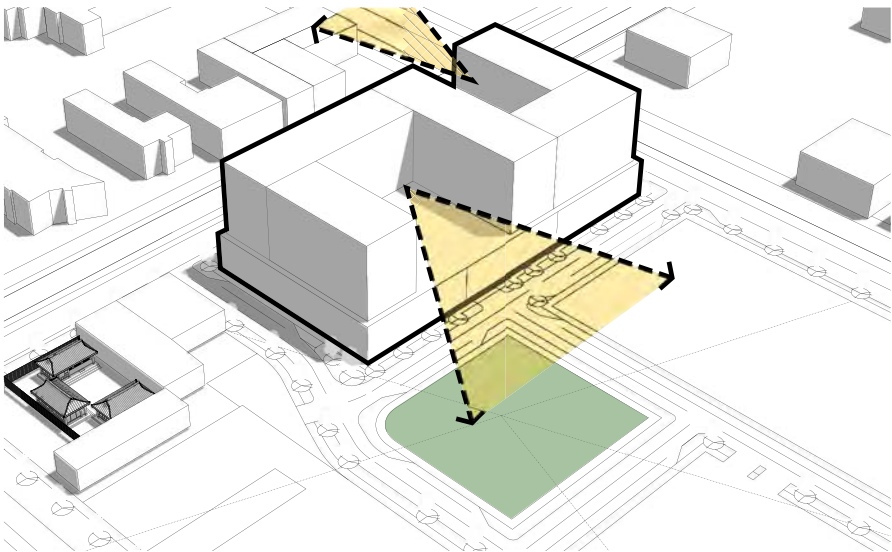
01 MAXIMIZE SITE ZONING ENVELOPE



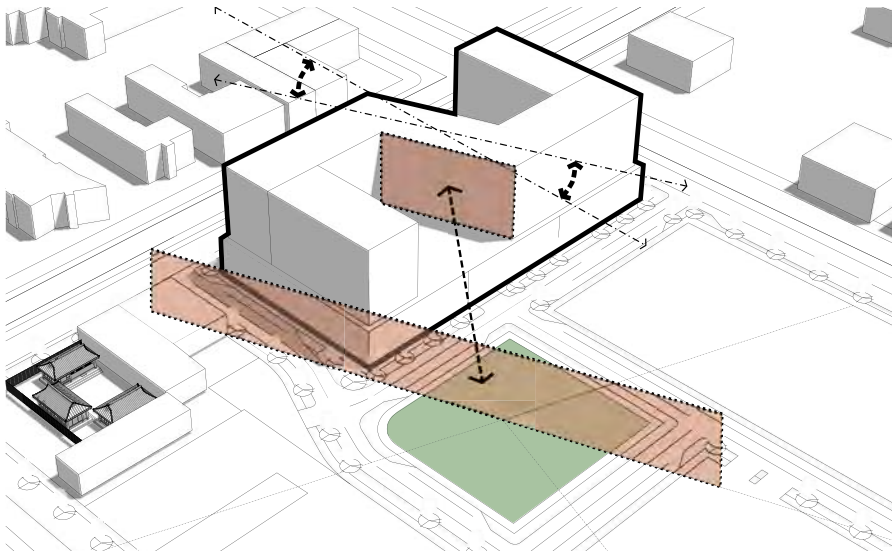
02 OFFSET UPPER FLOORS



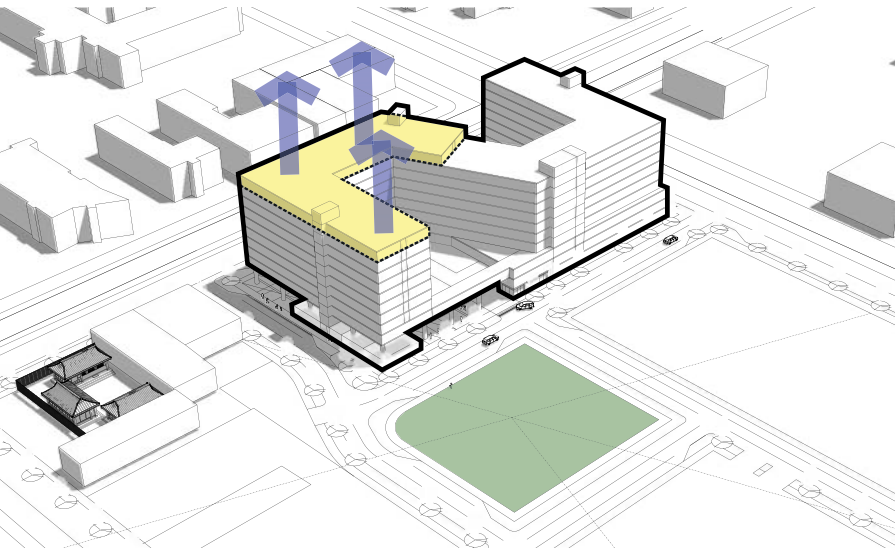
03 CREATE EAST/WEST COURTYARDS



04 DEFINE EAST/WEST VIEWS



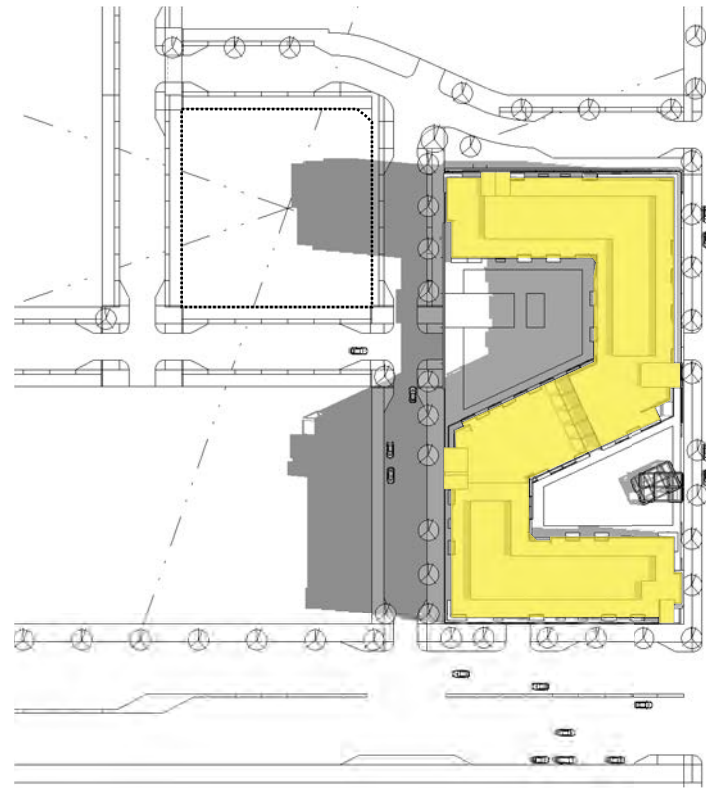
05 ALIGN MASS TO VIEW CORRIDOR



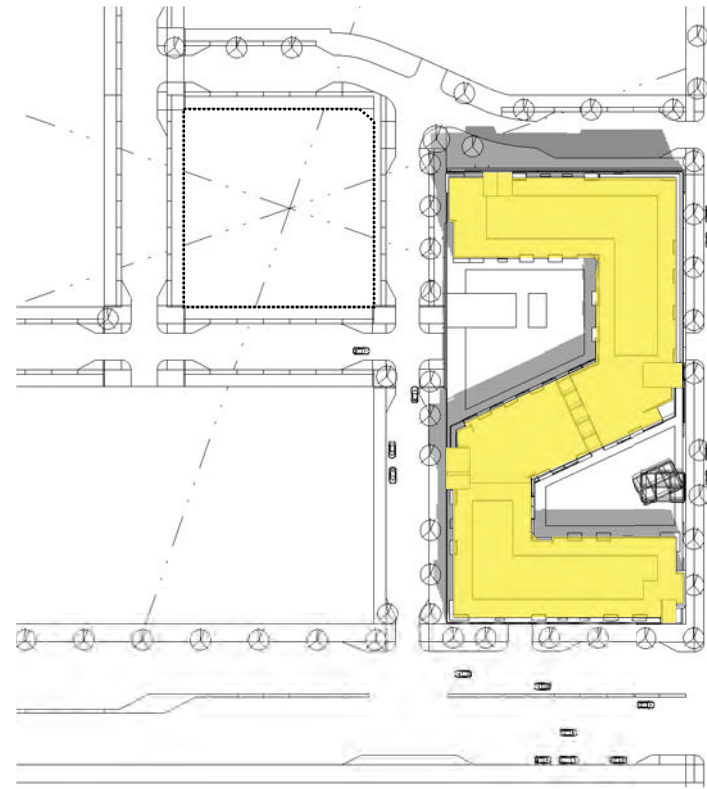
06 EXTRUDE PENTHOUSE



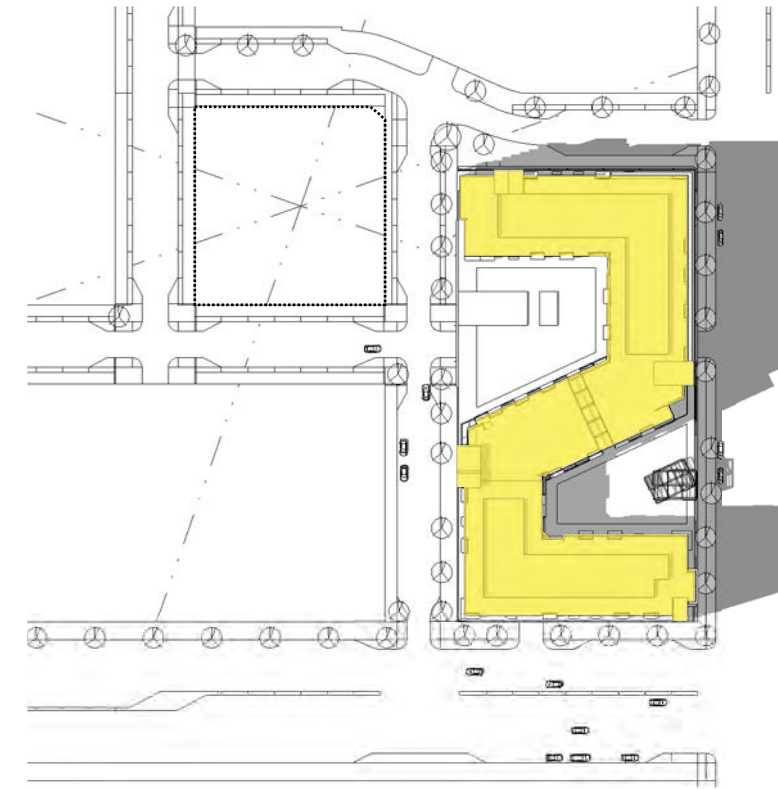
# SHADOW STUDY | PARCEL 3



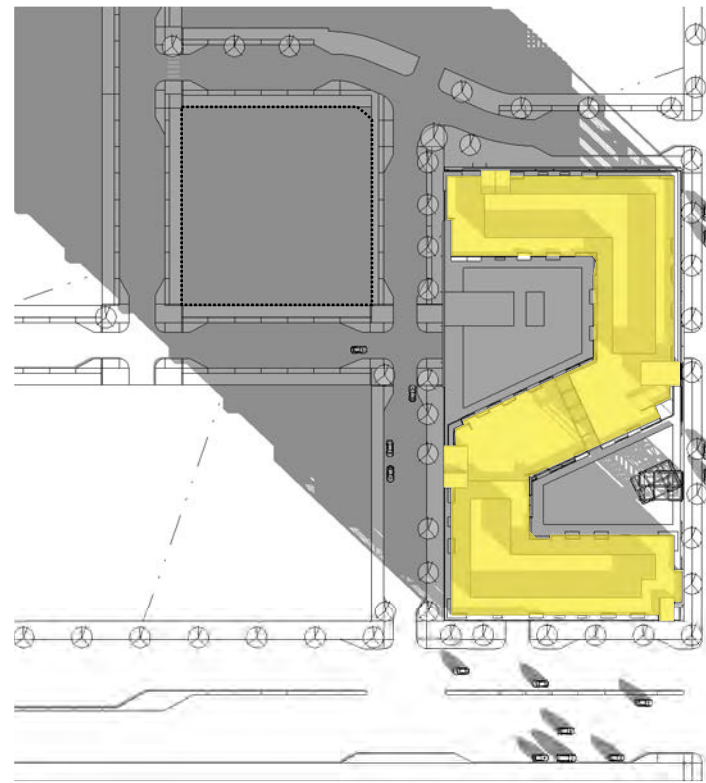
SUMMER (6/21) - 9AM



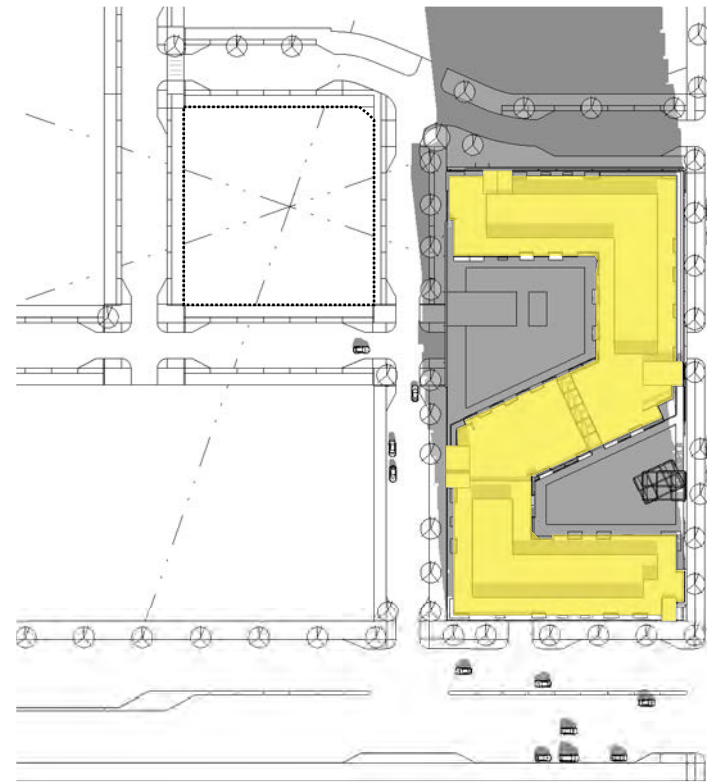
SUMMER (6/21) - 12PM



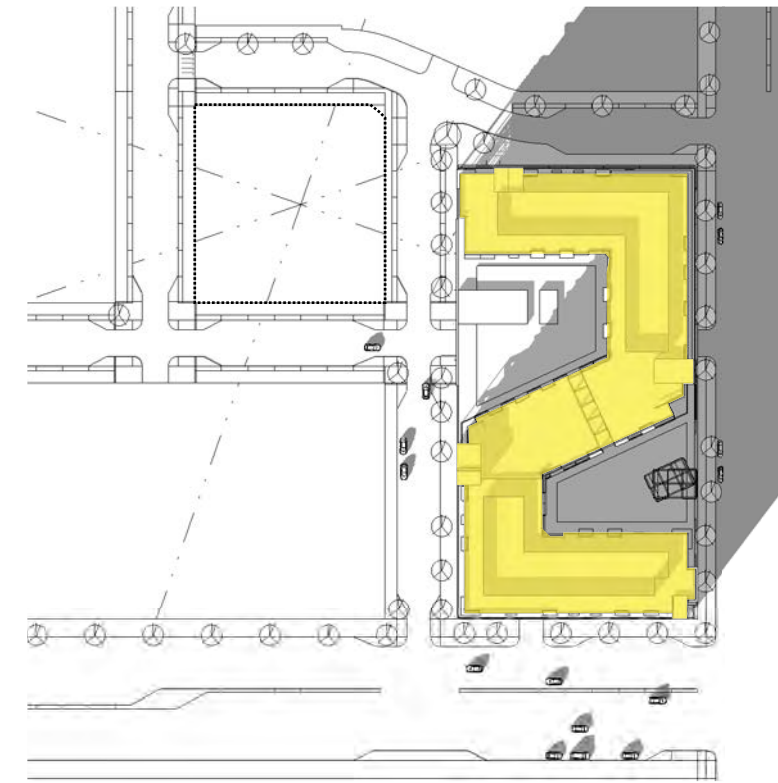
SUMMER (6/21) - 3PM



WINTER (12/21) - 9AM

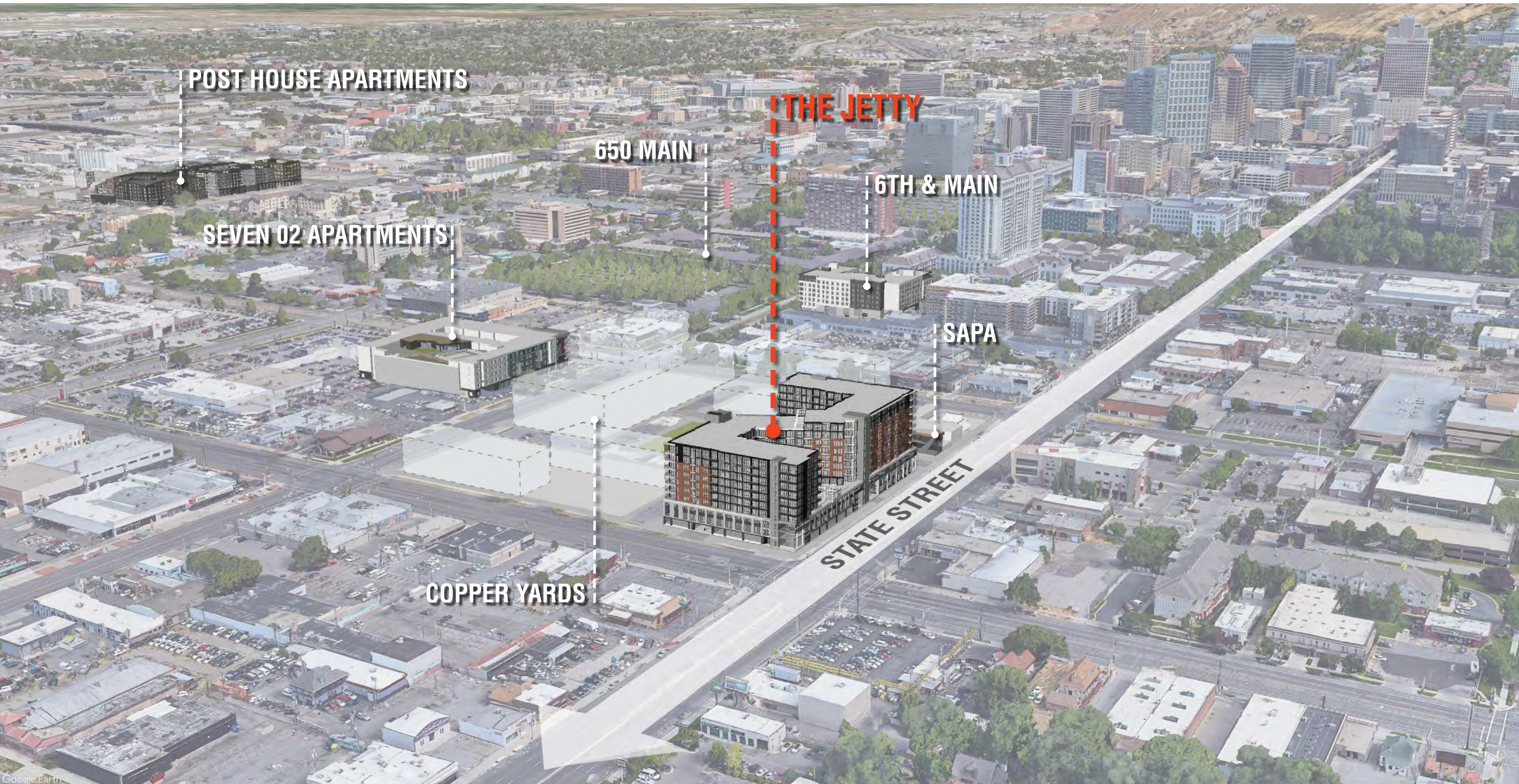


WINTER 12/21) - 12PM



WINTER (12/21) - 3PM





POST HOUSE APARTMENTS

SEVEN 02 APARTMENTS

COPPER YARDS

650 MAIN

THE JETTY

6TH & MAIN

SAPA

STATE STREET



# AUGMENT THE URBAN FABRIC.

Create, repair, and enhance connections within existing neighborhoods.

# MAKE BIG MOVES.

Design bold, interesting building forms, create a new icon.

**MUCH MORE THAN SIMPLY DESIGNING BUILDINGS FOR PEOPLE TO LIVE IN...**

**GOOD URBAN DESIGN BUILDS COMMUNITIES.**

# BE WELCOMING.

Set a positive tone with a bright, engaging entryway.

# CULTIVATE CONNECTION.

Place compatible uses together to add convenience and support social encounters.

# ENLIGHTENED CIRCULATION.

Bring light and fresh air into hallways and stairs – connect with nature and encourage walking.

# GET PERSONAL.

Echo character of the evolving community. Listen to what is around you.

# A LITTLE GOES A LONG WAY.

Concentrate premium materials at points of shared enjoyment – keep it simple everywhere else.

# ACTIVATE THE EDGES.

Energize the street scape with a generous, mixed-use ground floor.

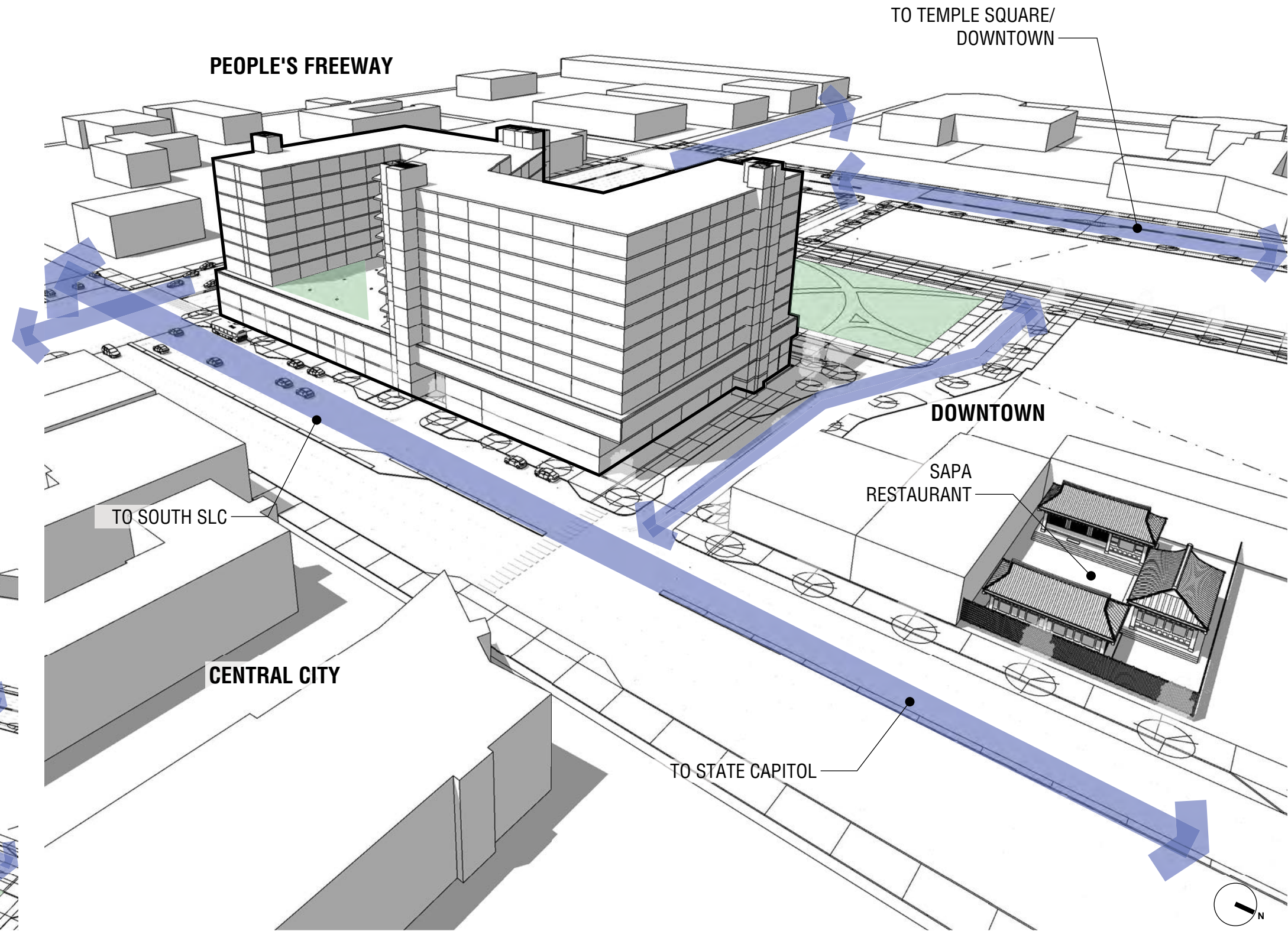
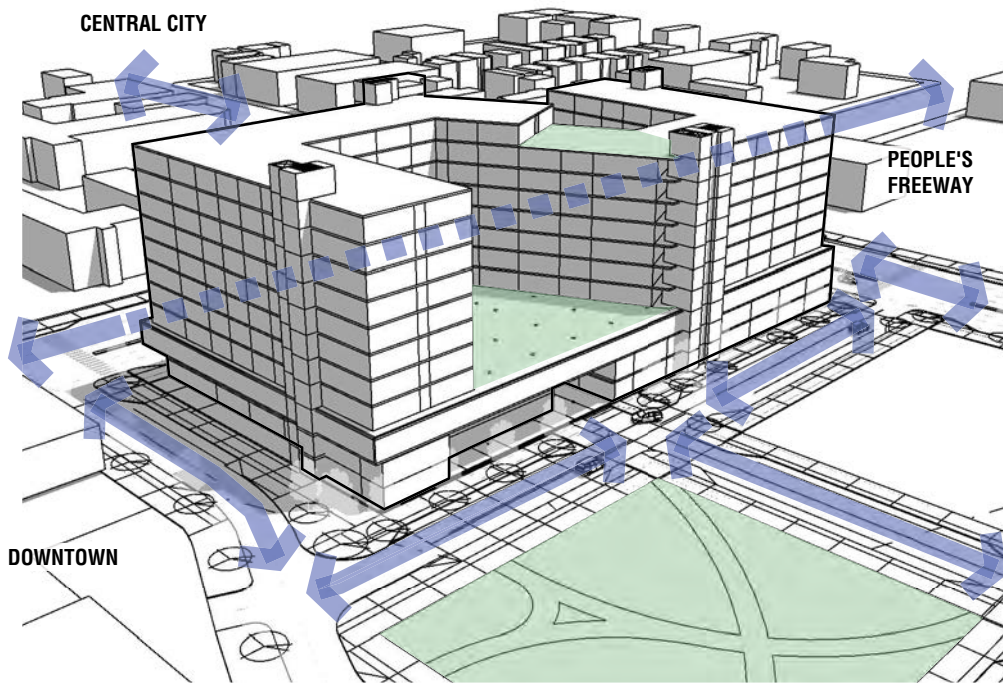
# ART FOR ALL.

Reflect the character of the community and offer opportunities for expression.



# 01. AUGMENT THE URBAN FABRIC

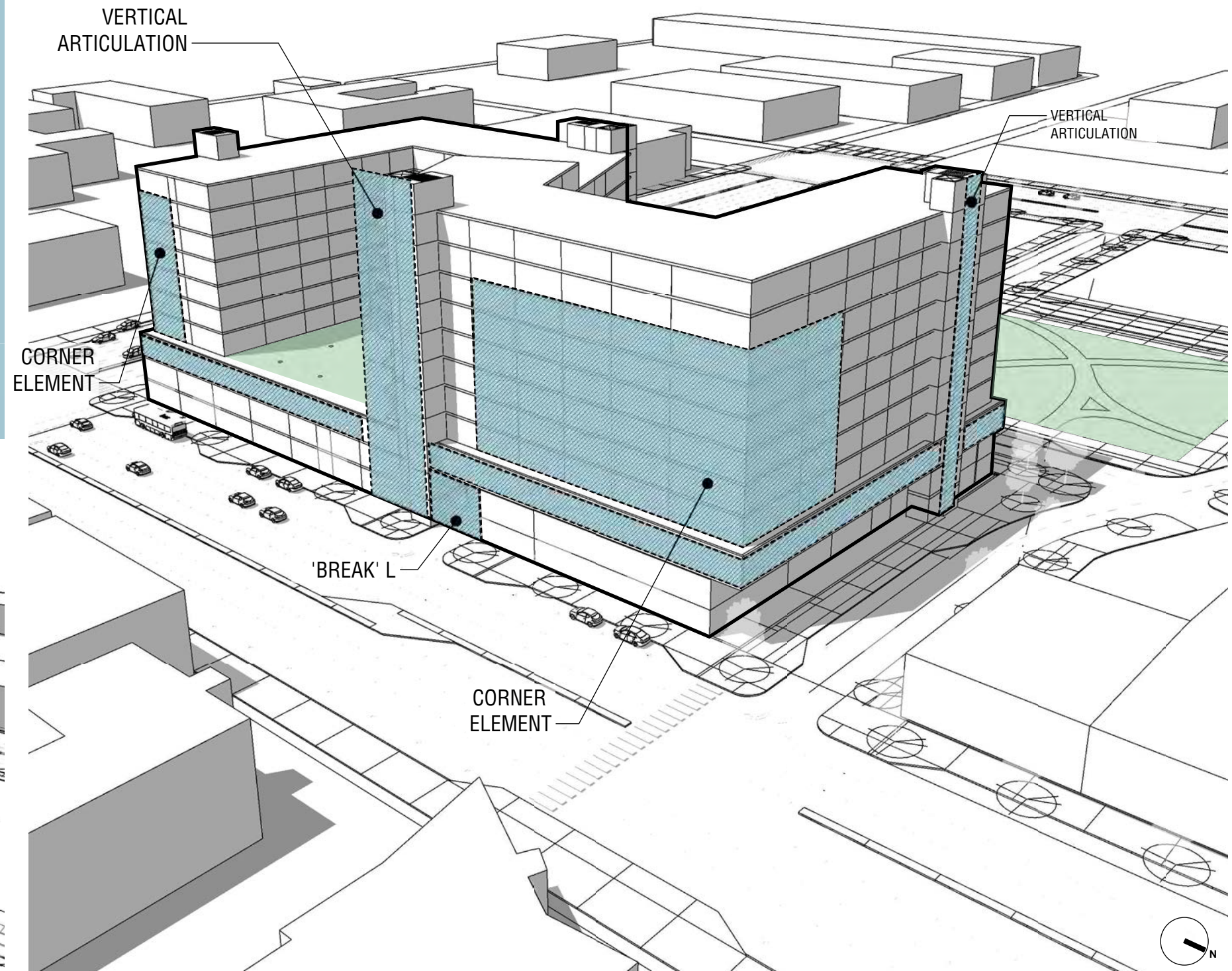
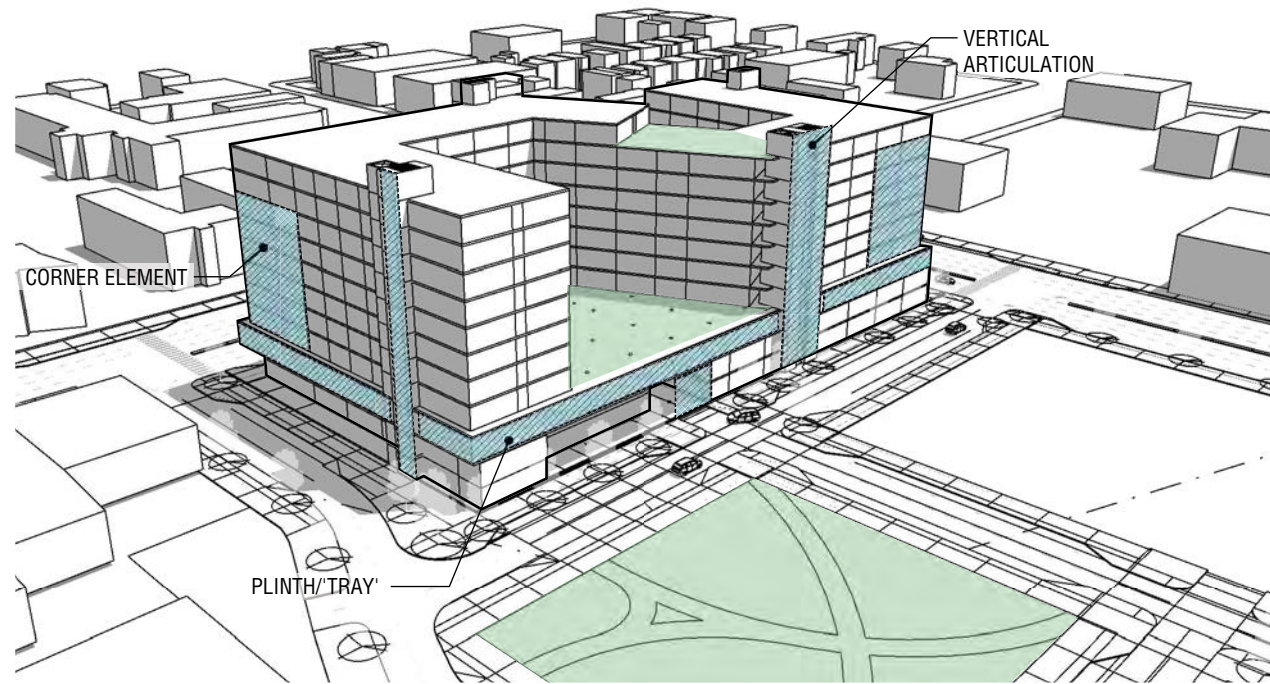
CREATE, REPAIR, AND ENHANCE  
CONNECTINS WITHIN EXISTING  
NEIGHBORHOODS.



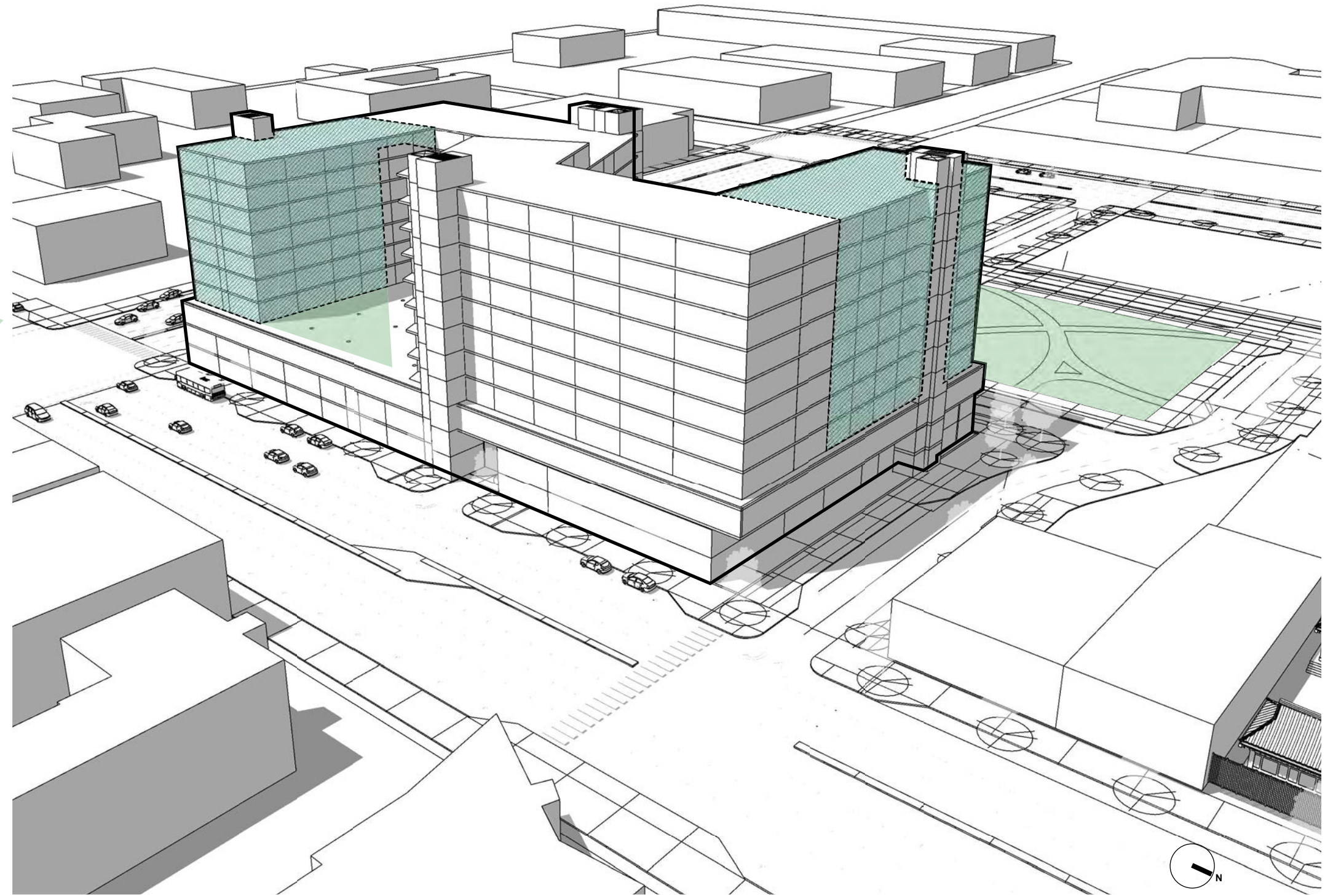
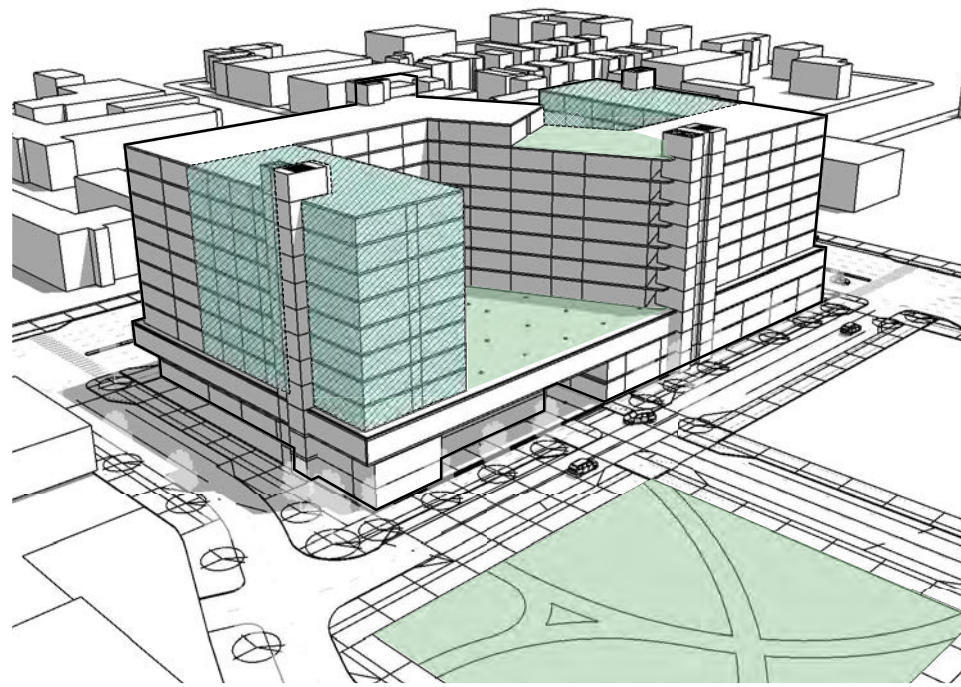


# 02. MAKE BIG MOVES

DESIGN BOLD, INTERESTING BUILDING FORMS, CREATE A NEW ICON.



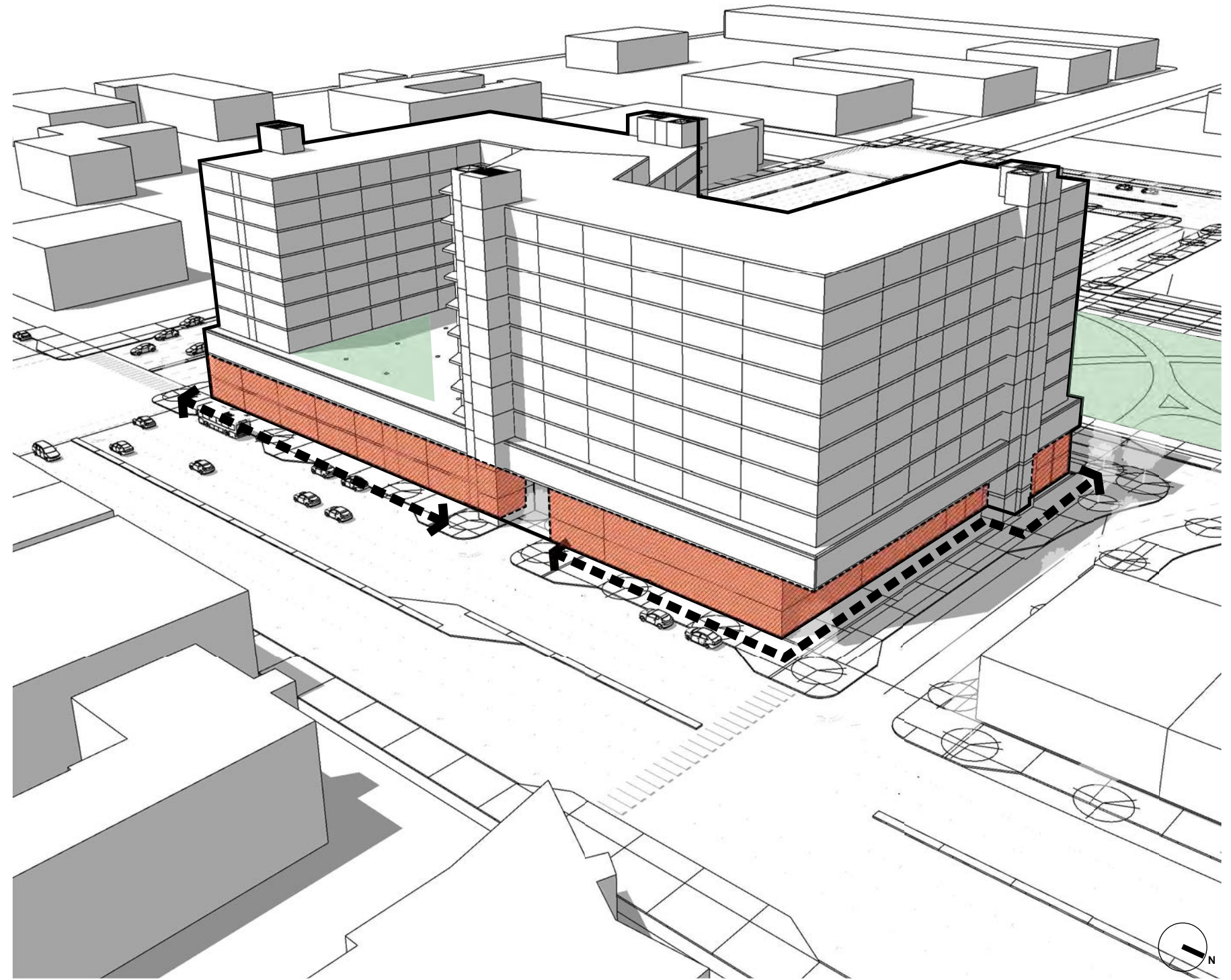
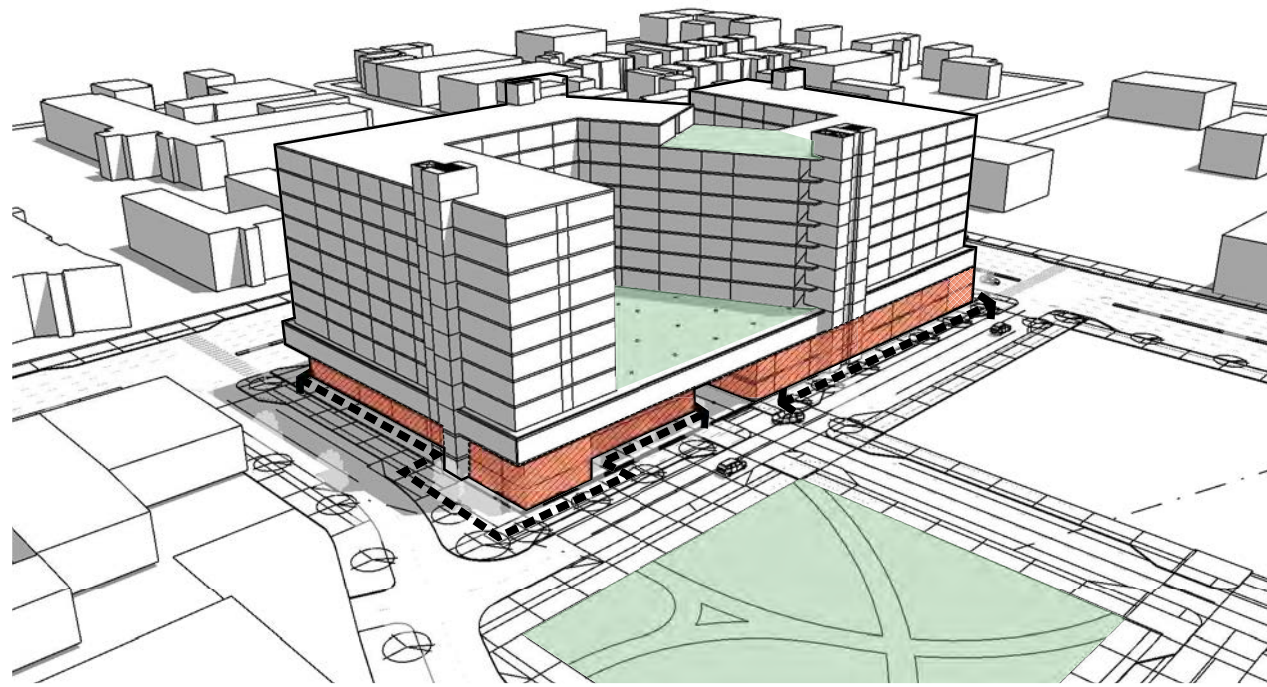




03.  
A LITTLE  
GOES A  
LONG WAY

CONCENTRATE PREMIUM MATERIALS  
AT POINTS OF SHARED ENJOYMENT -  
KEEP IT SIMPLE EVERYWHERE ELSE.





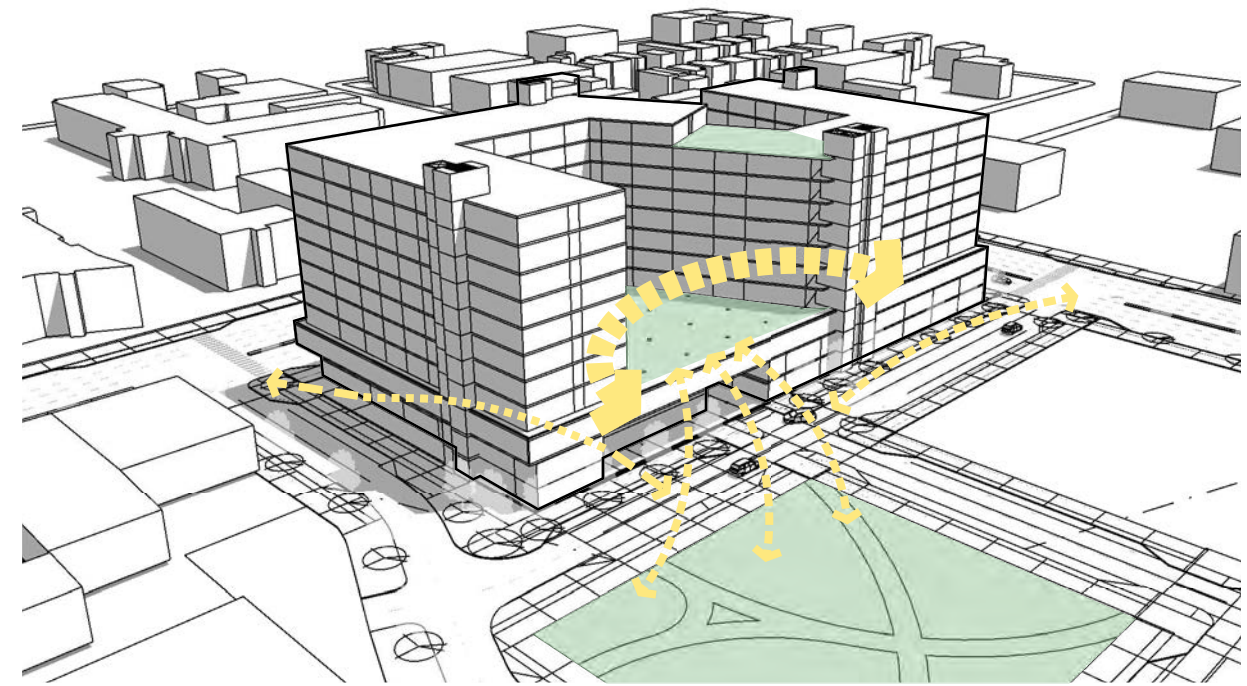
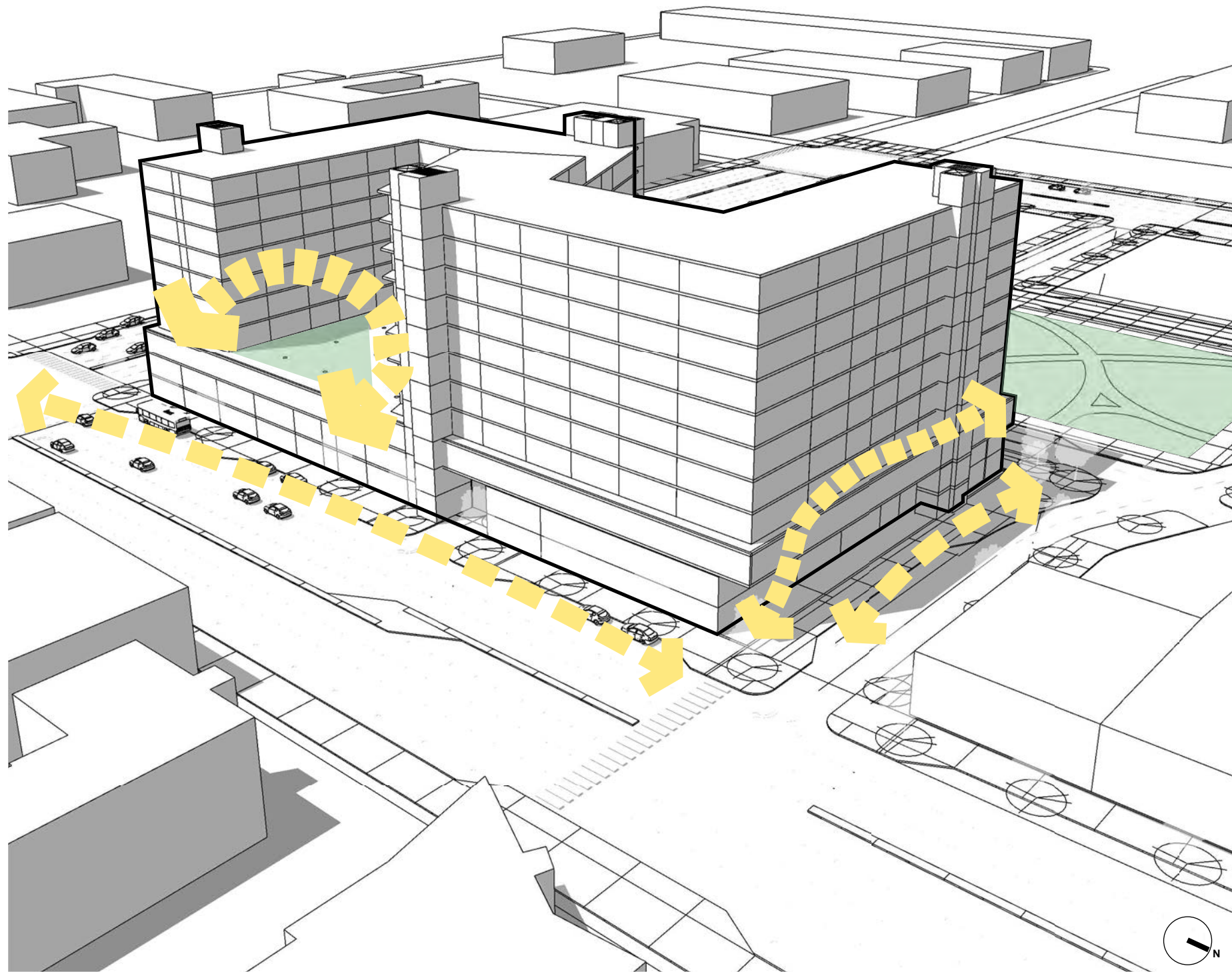
## 04. ACTIVATE THE EDGES

ENERGIZE THE STREET SCAPE WITH A  
GENEROUS, MIXED-USE GROUND  
FLOOR.



05.  
BE  
WELCOMING

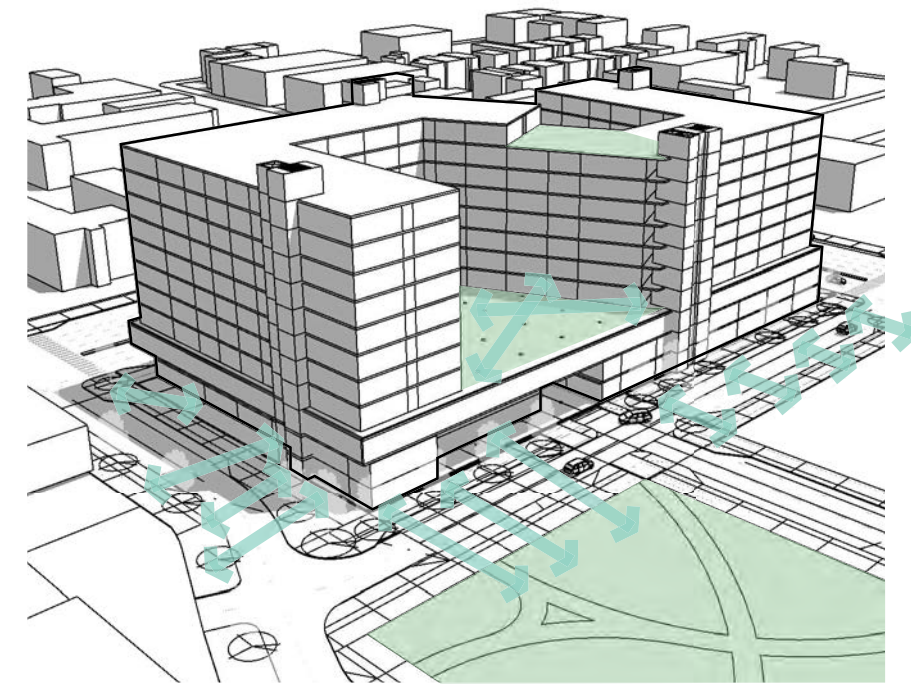
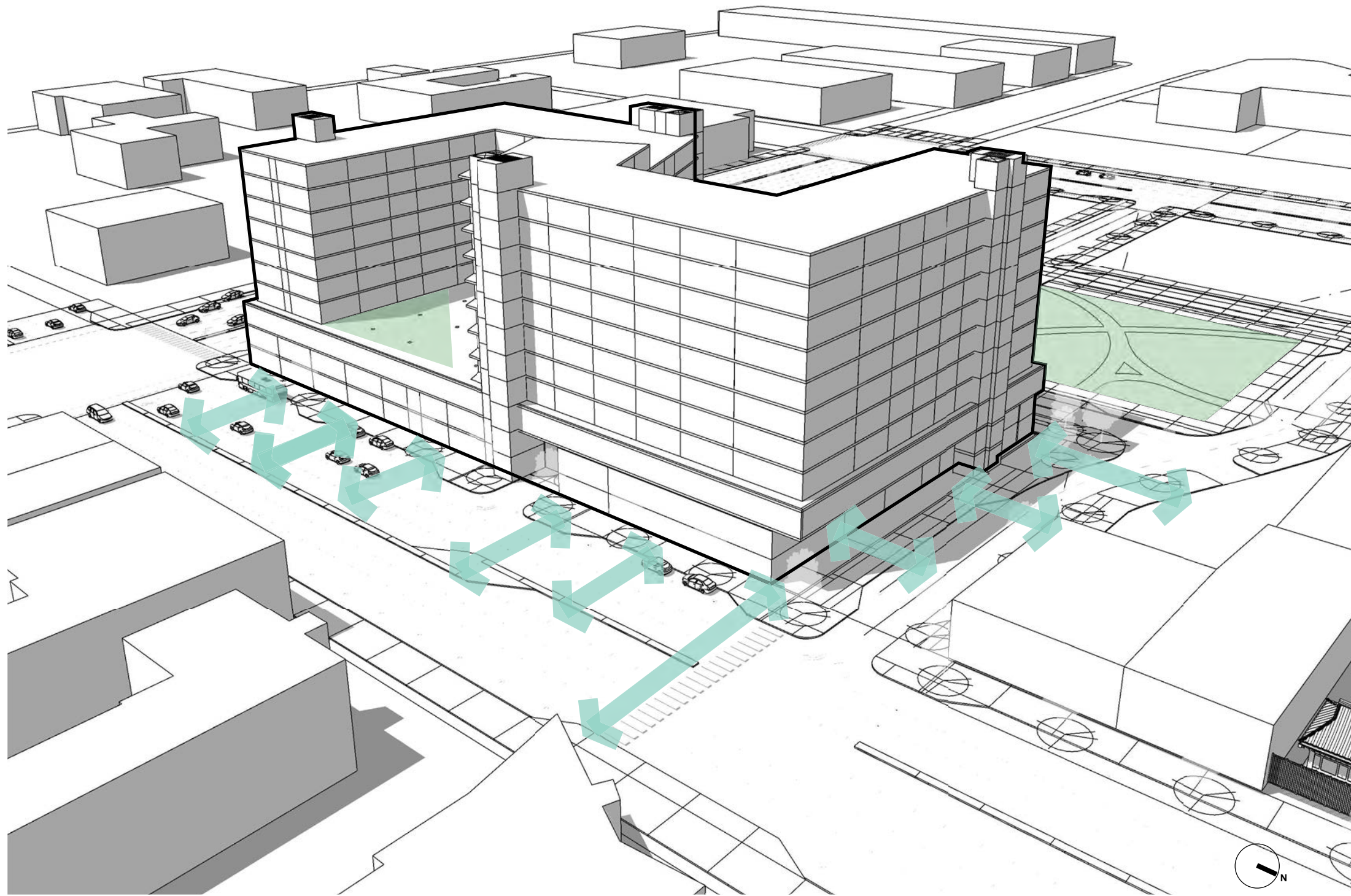
SET A POSITIVE TONE WITH A BRIGHT,  
ENGAGING ENTRYWAY.



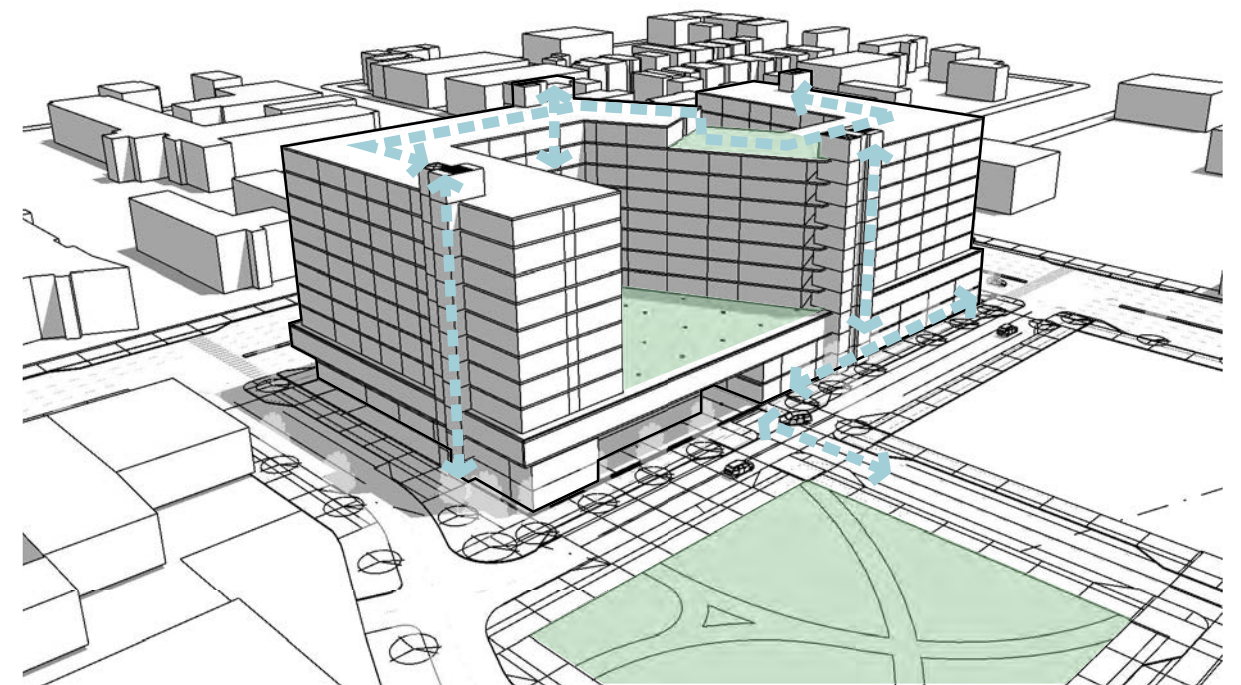
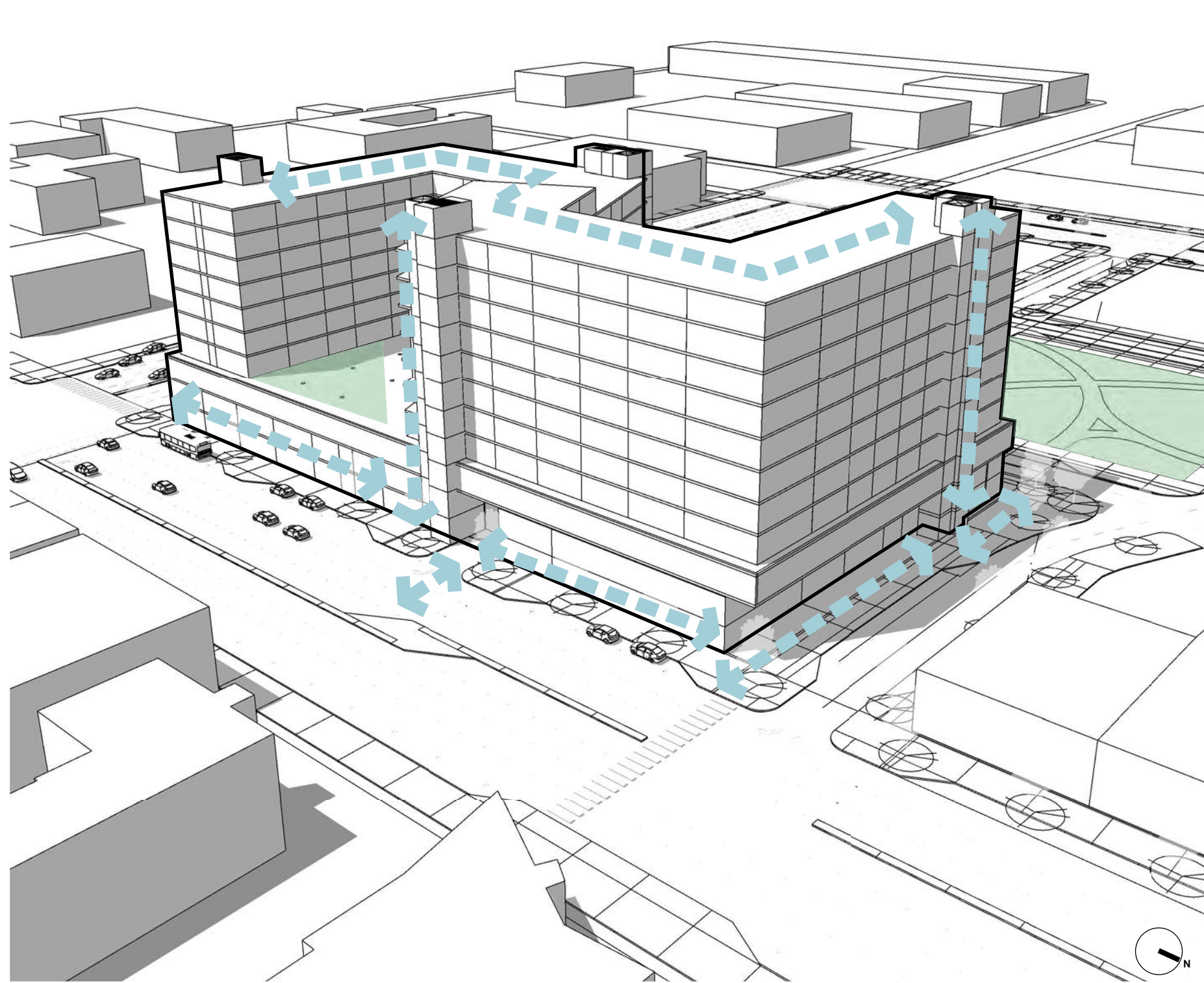


06.  
CULTIVATE  
CONNECTIONS

PLACE COMPATIBLE USES TOGETHER  
TO ADD CONVENIENCE AND SUPPORT  
SOCIAL ENCOUNTERS.



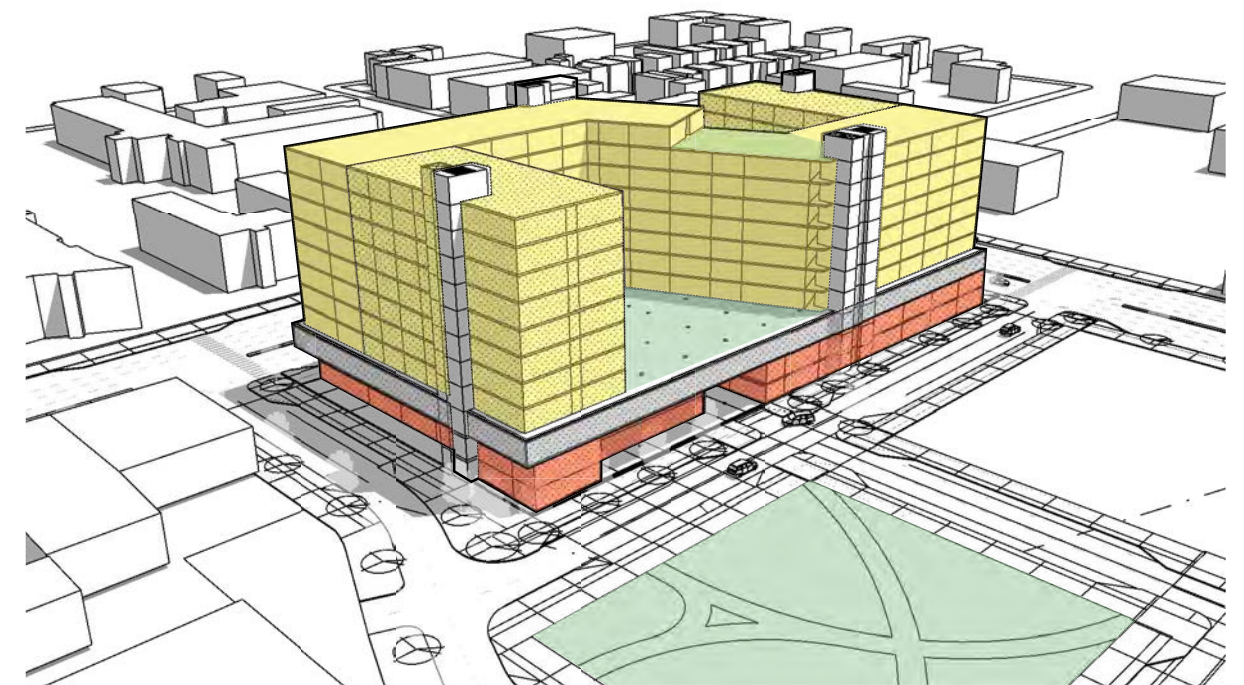
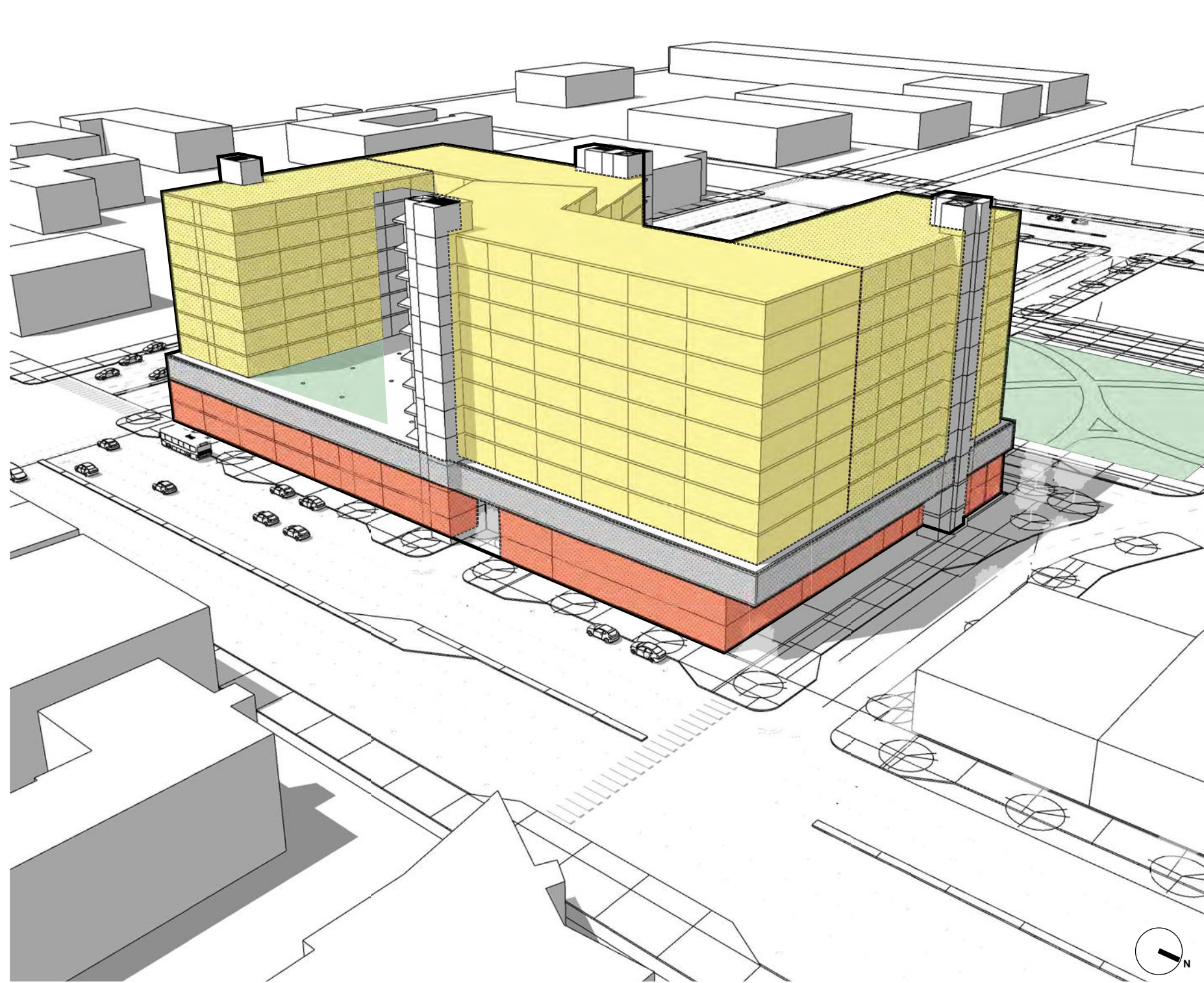




## 07 ENLIGHTENED CIRCULATION

BRING LIGHT AND FRESH AIR INTO  
HALLWAYS AND STAIRS - CONNECT  
WITH NATURE AND ENCOURAGE  
WALKING.

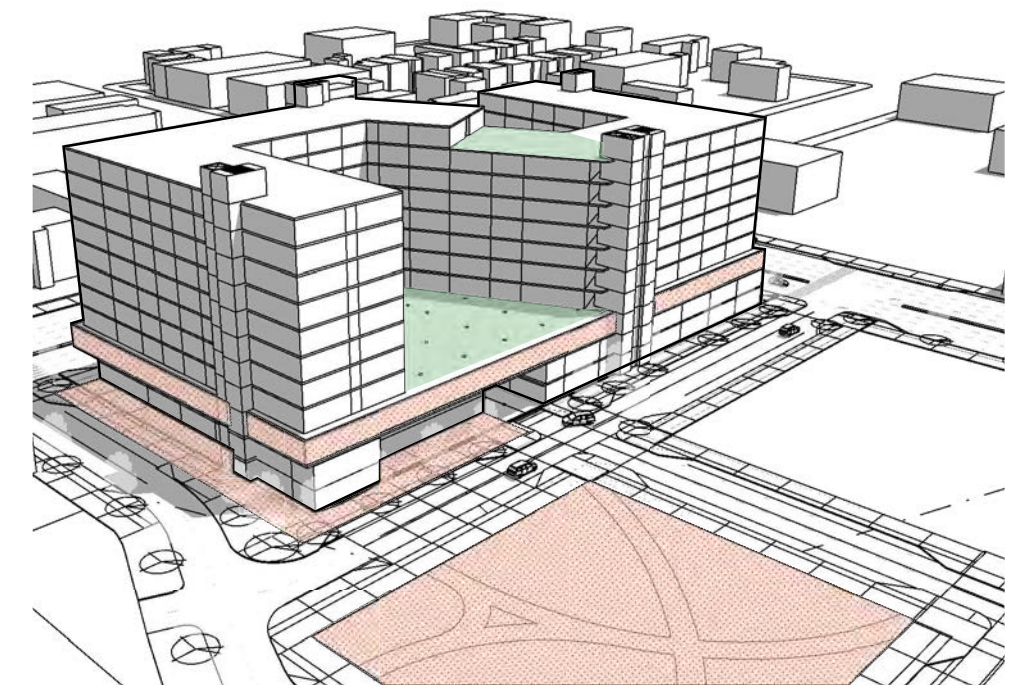
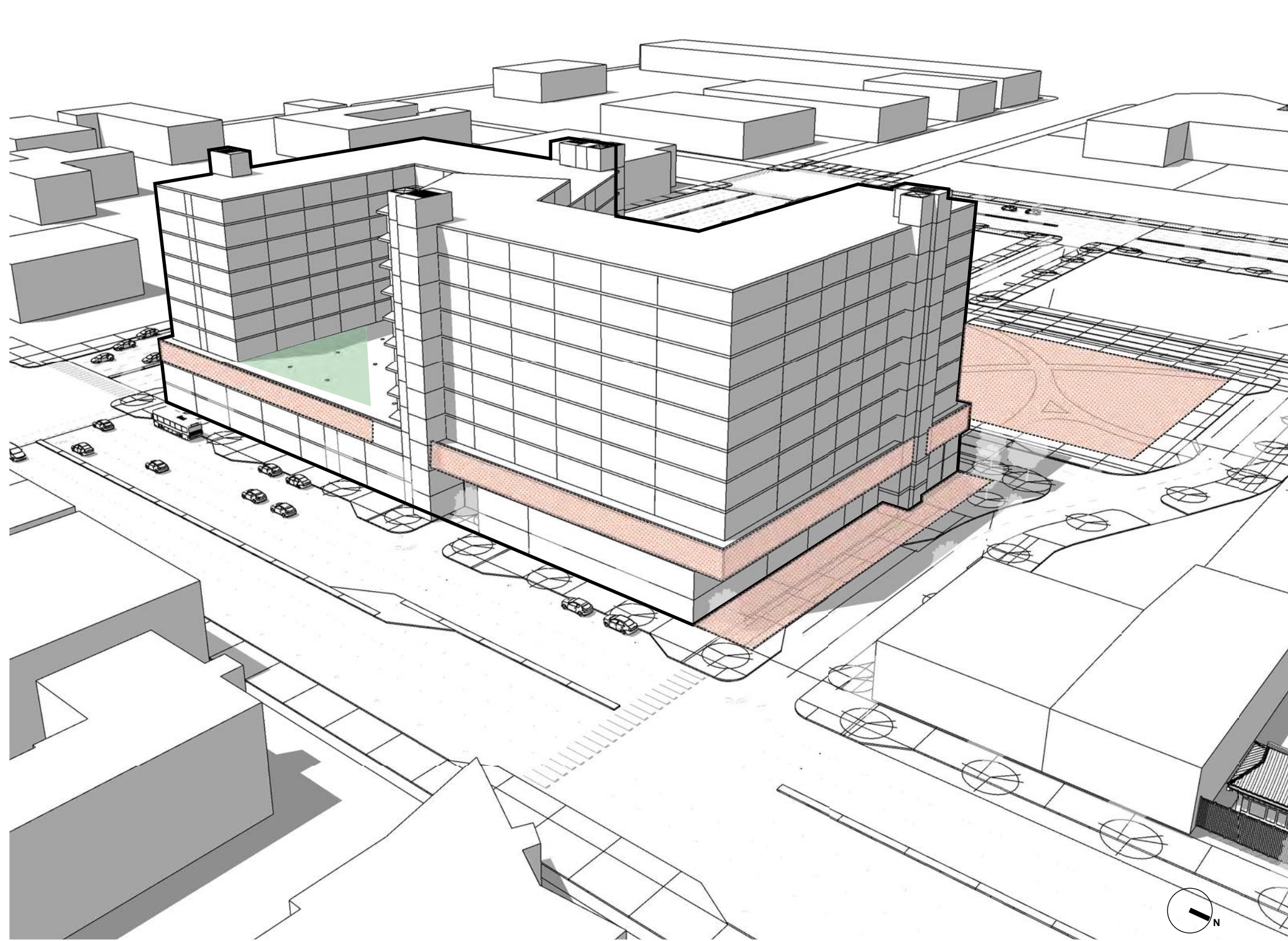




## 08 GET PERSONAL

ECHO THE CHARACTER OF THE  
COMMUNITY. LISTEN TO WHAT IS  
AROUND YOU.



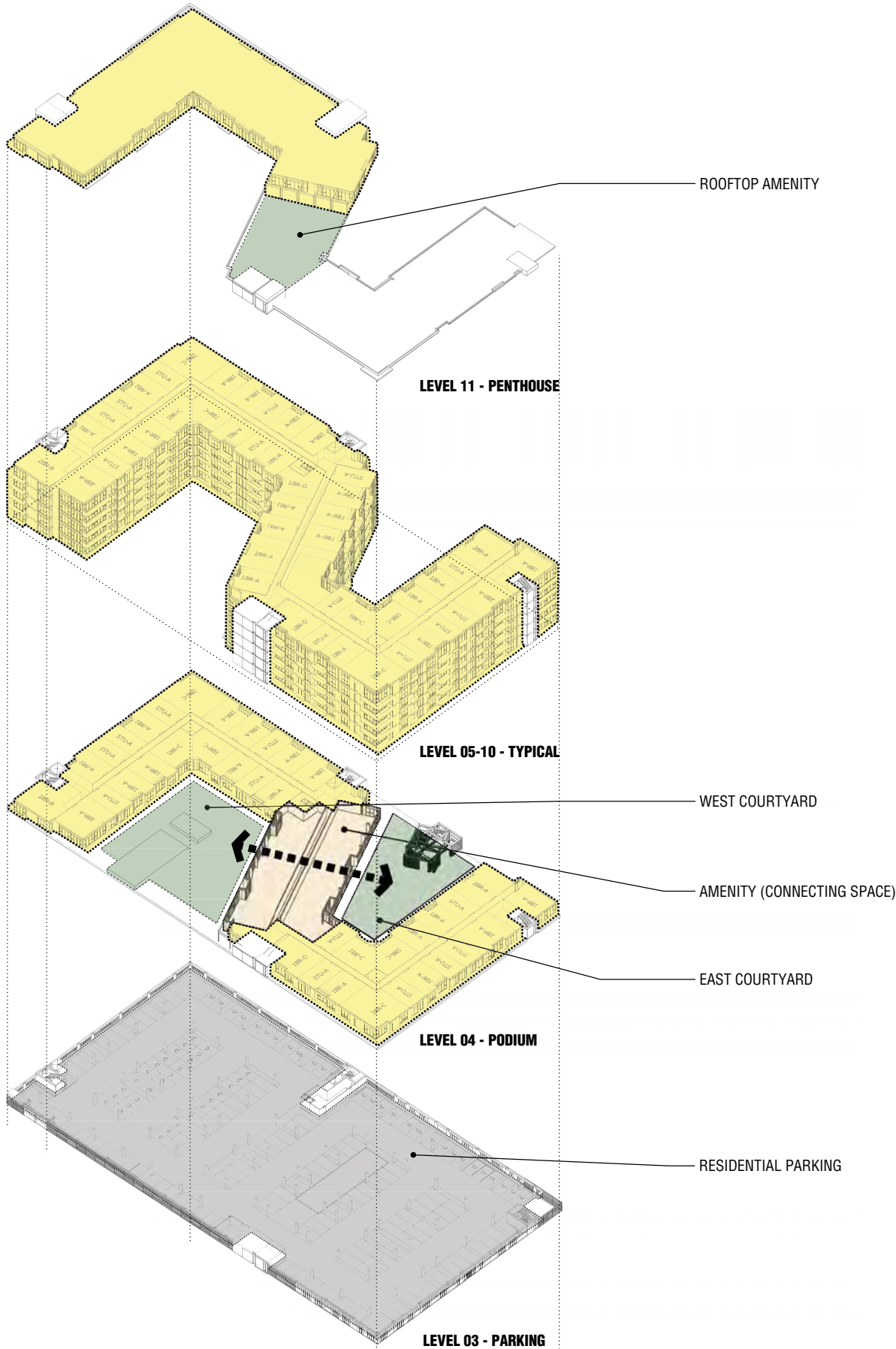
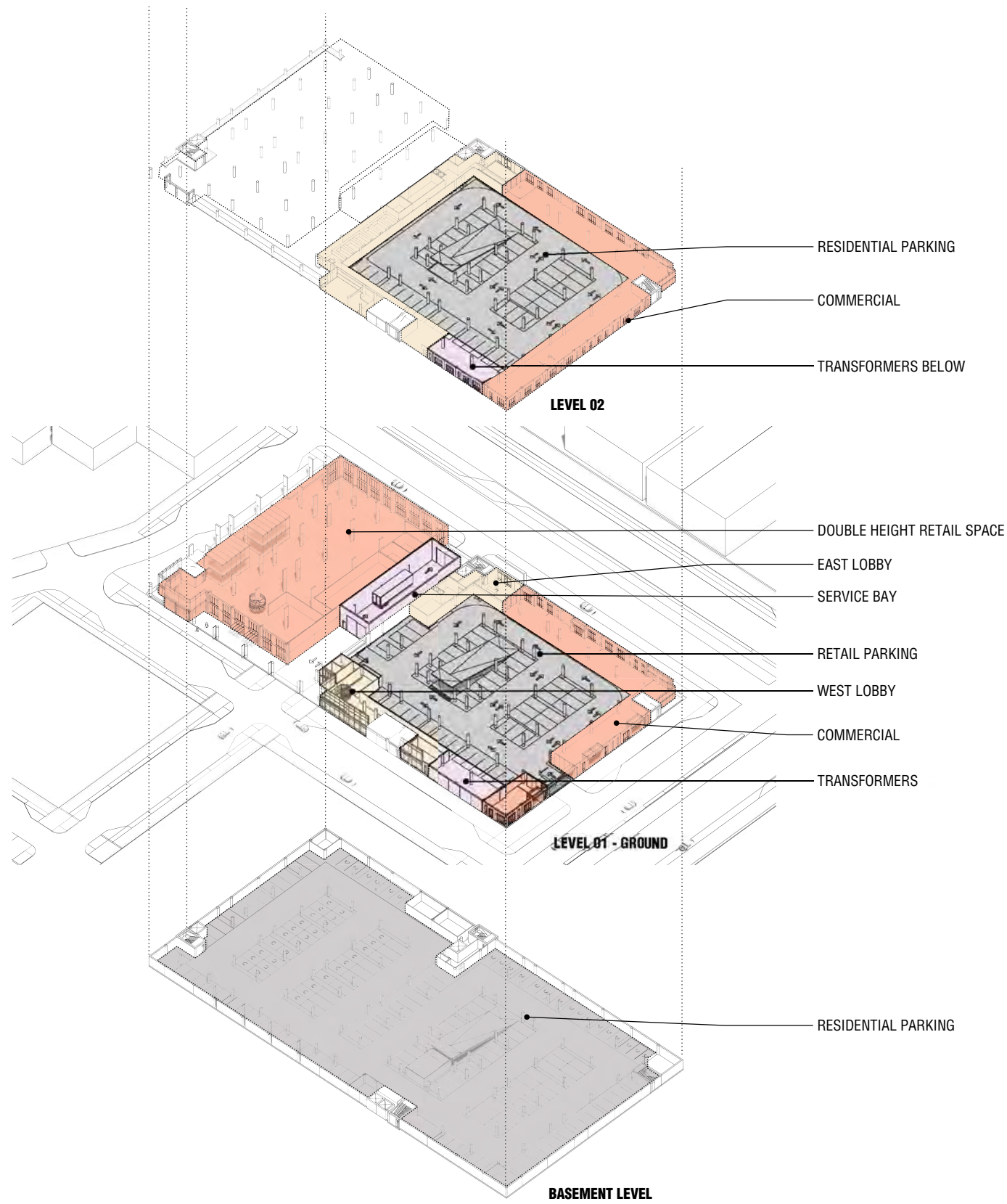


## 09. ART FOR ALL

REFLECT THE CHARACTER OF THE  
COMMUNITY AND OFFER  
OPPORTUNITIES FOR EXPRESSION.

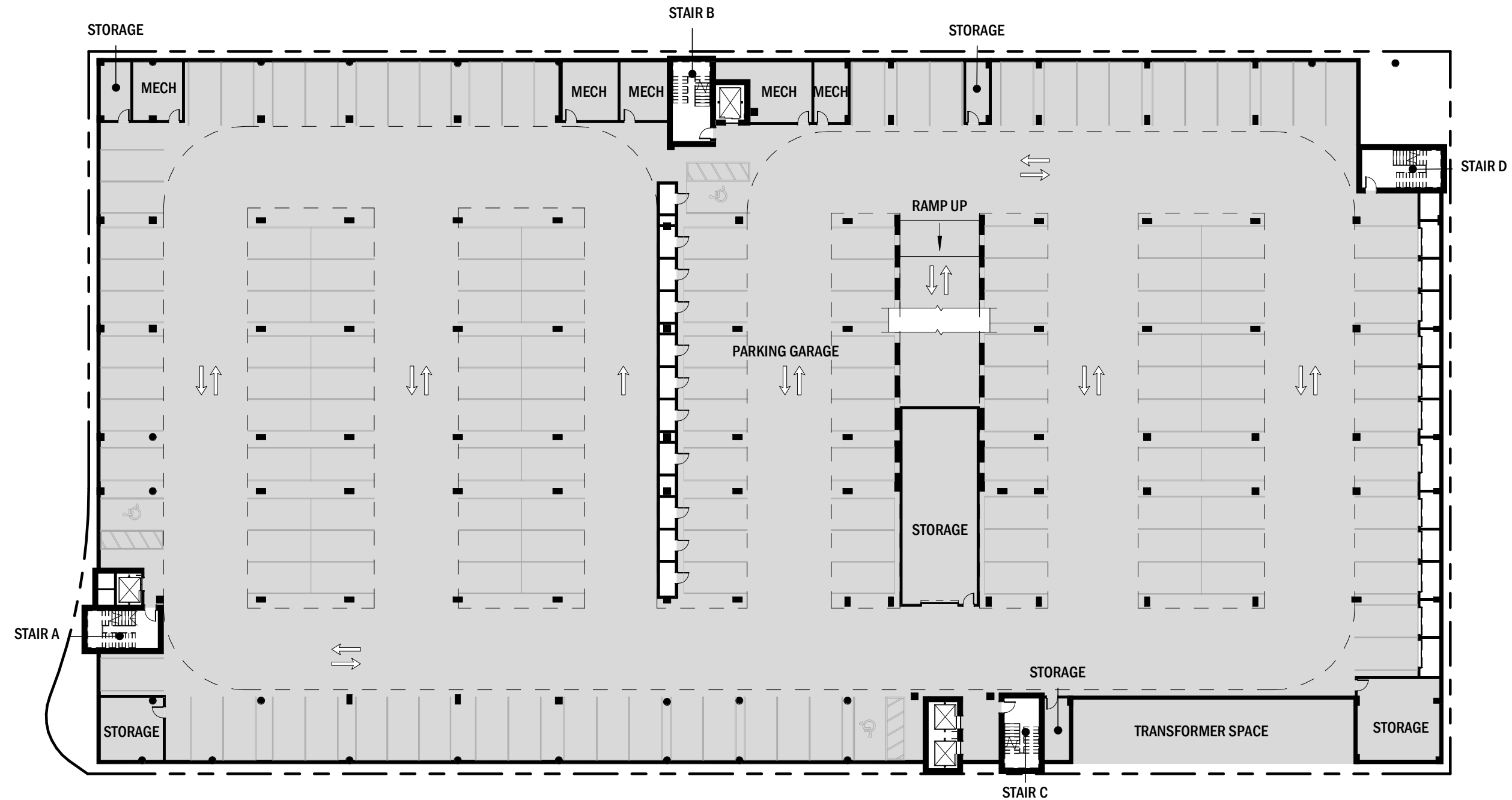


# EXPLODED AXON | BUILDING LAYOUT





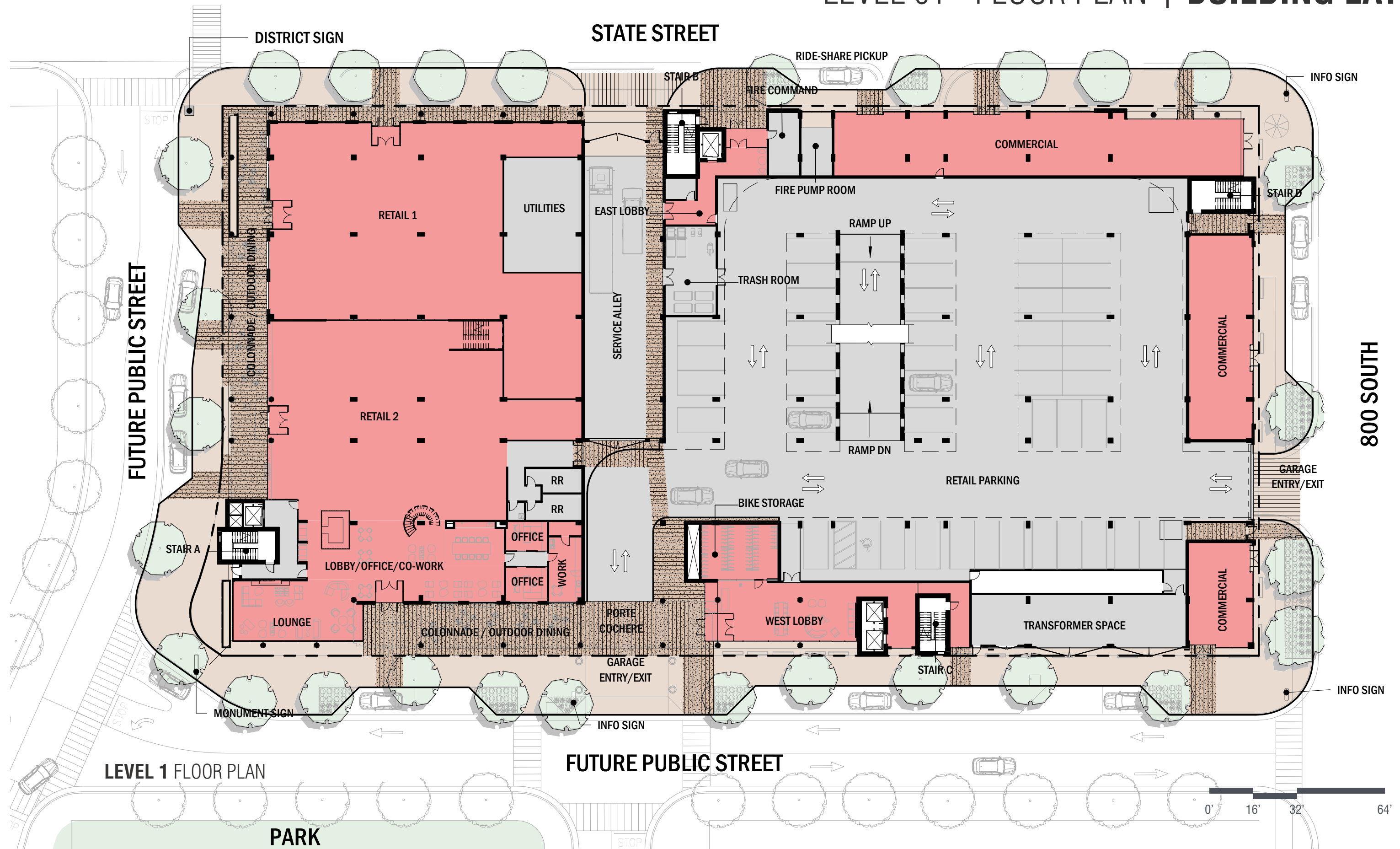
# LEVEL B1 - BASEMENT PLAN | BUILDING LAYOUT



LEVEL B1 BASEMENT PLAN



# LEVEL 01 - FLOOR PLAN | BUILDING LAYOUT



LEVEL 1 FLOOR PLAN

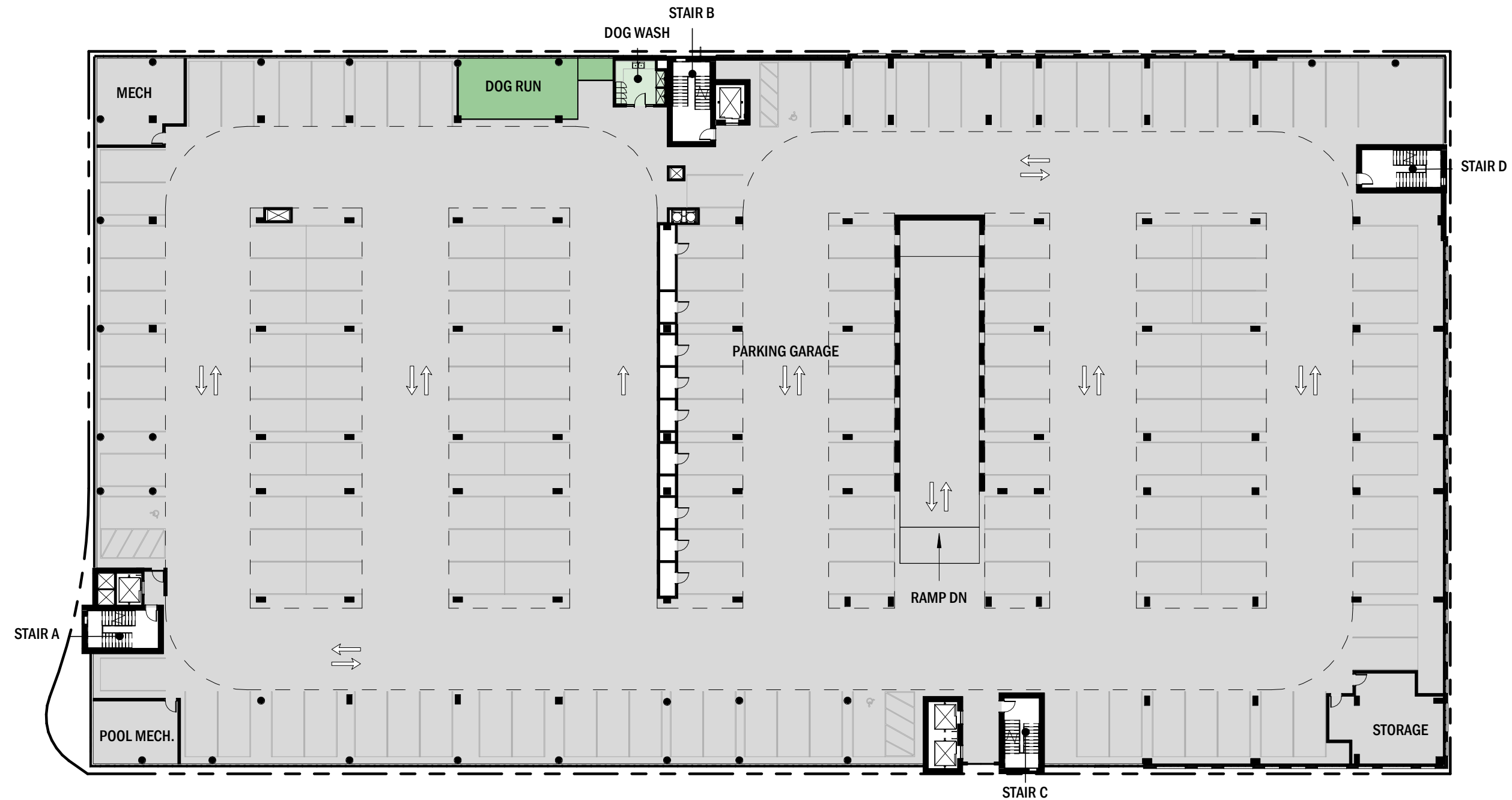


# LEVEL 02 - FLOOR PLAN | BUILDING LAYOUT



LEVEL 2 FLOOR PLAN

# LEVEL 03 - FLOOR PLAN | BUILDING LAYOUT

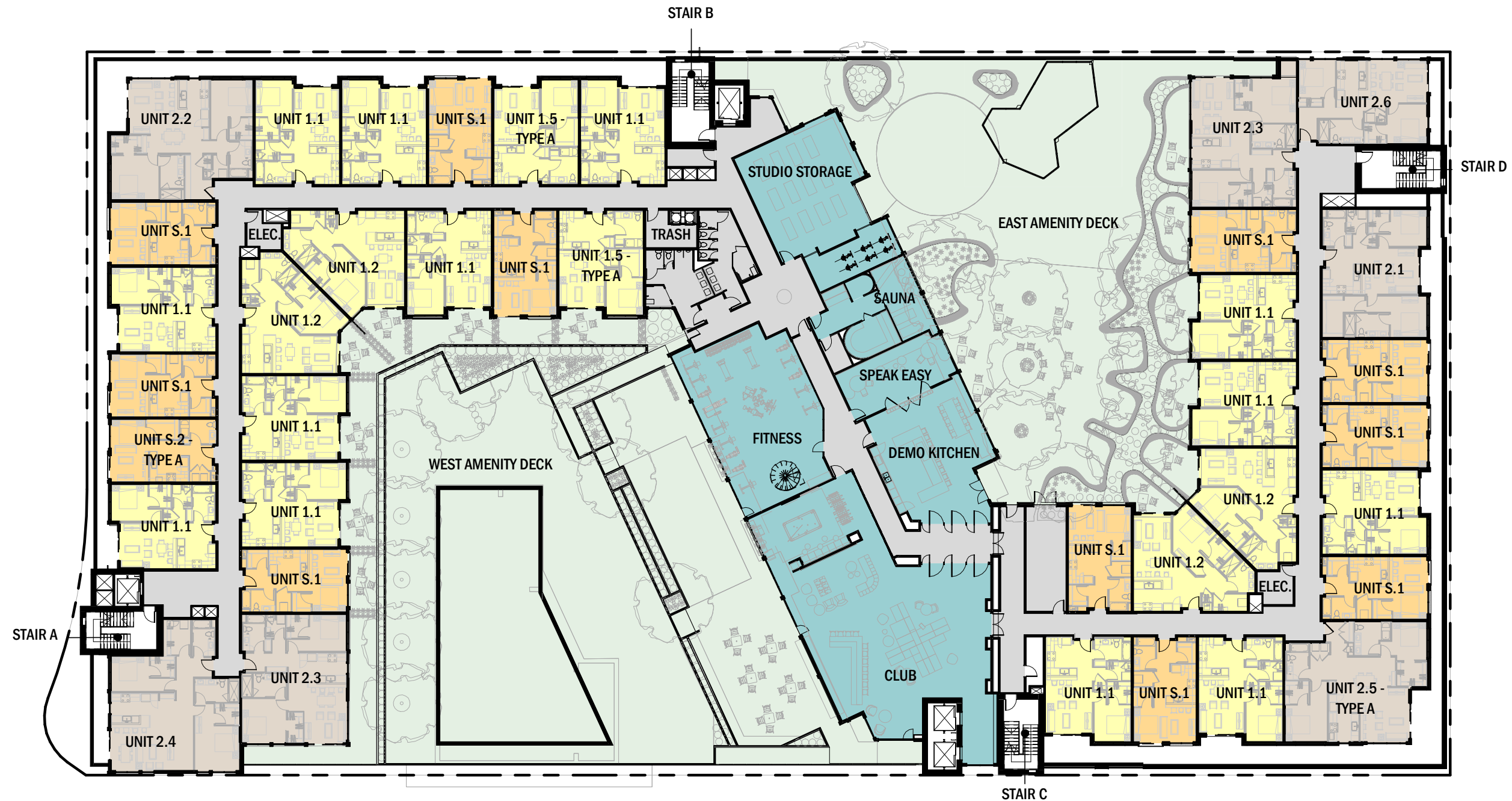


LEVEL 3 FLOOR PLAN





# LEVEL 04 - FLOOR PLAN | BUILDING LAYOUT

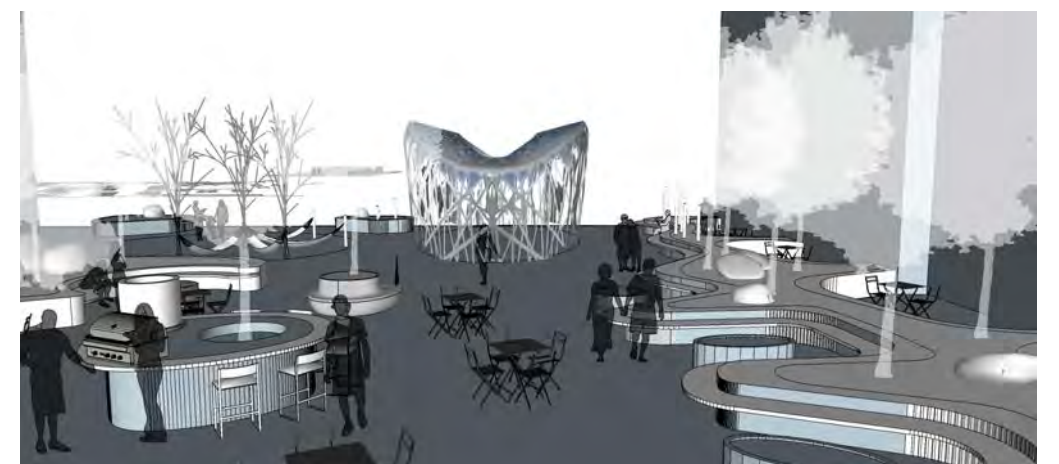
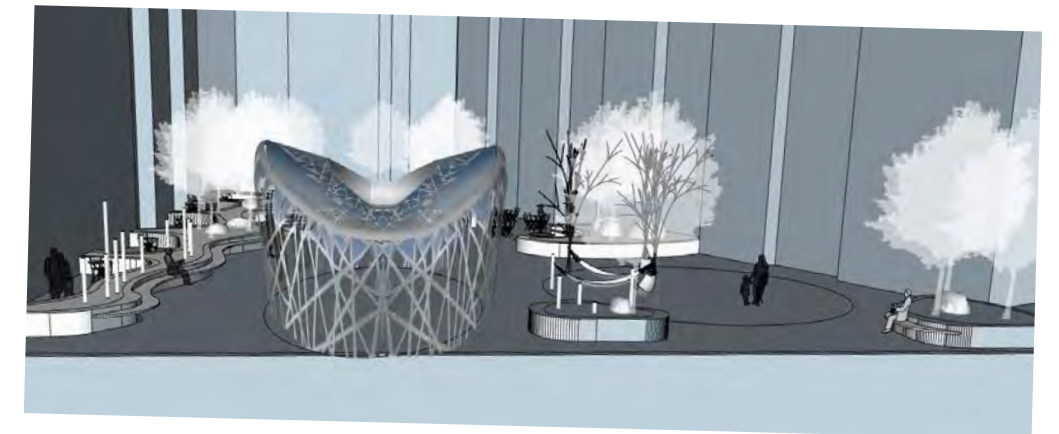
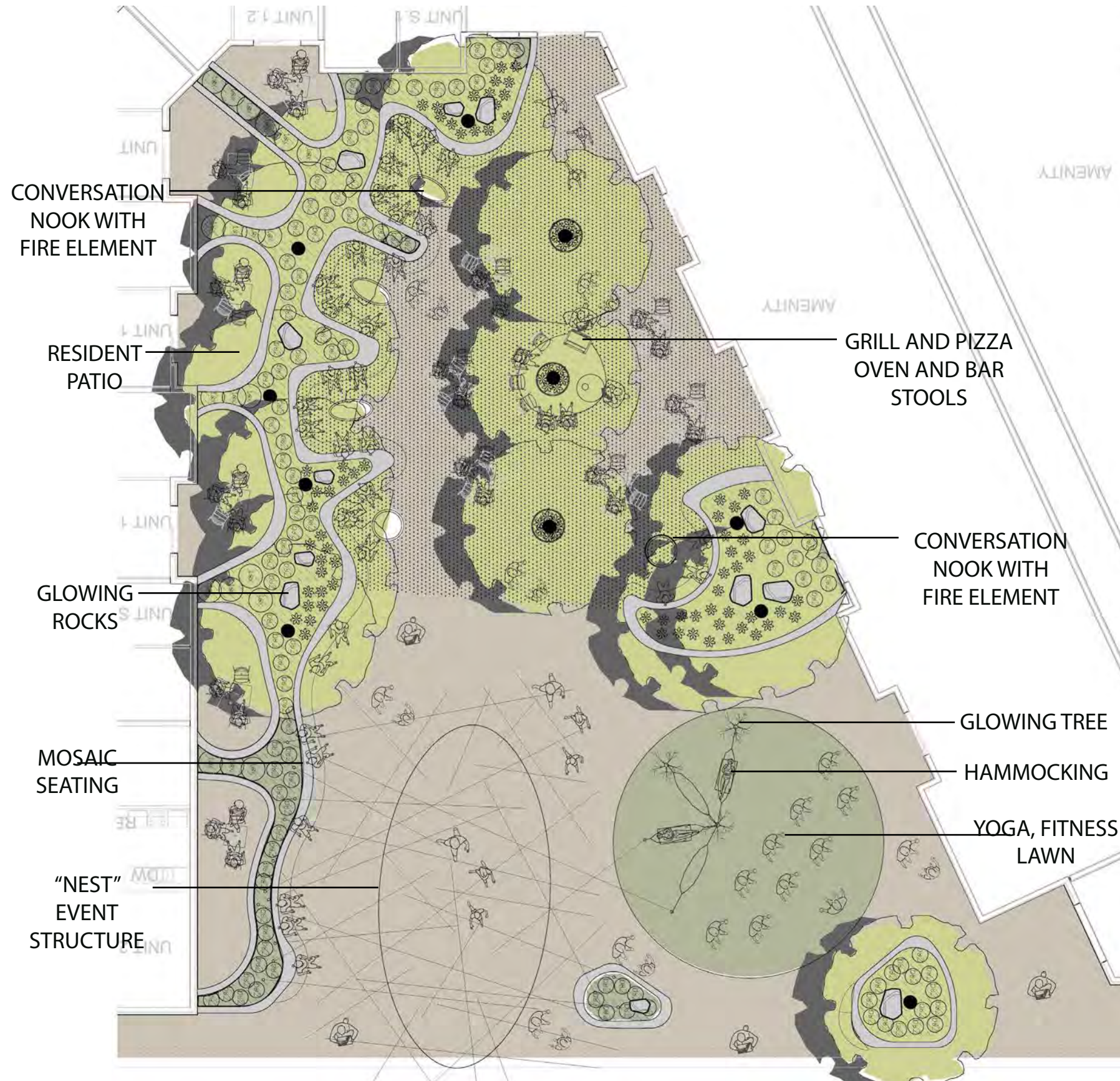


LEVEL 4 FLOOR PLAN



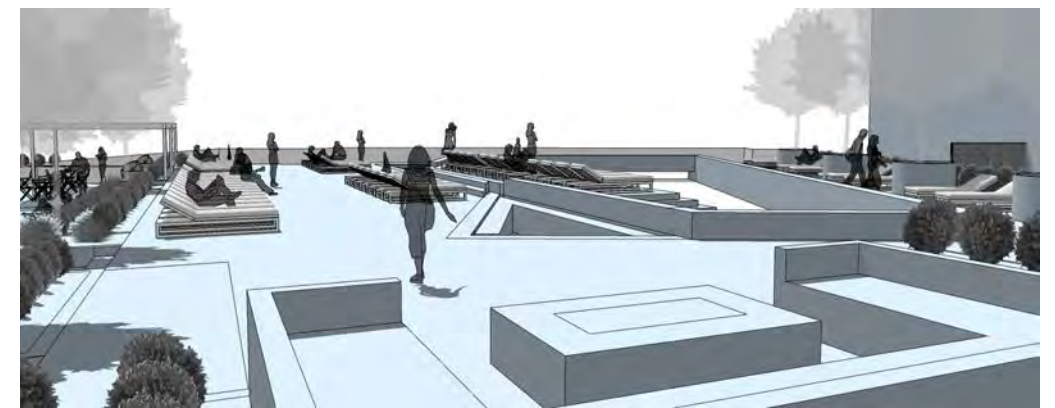
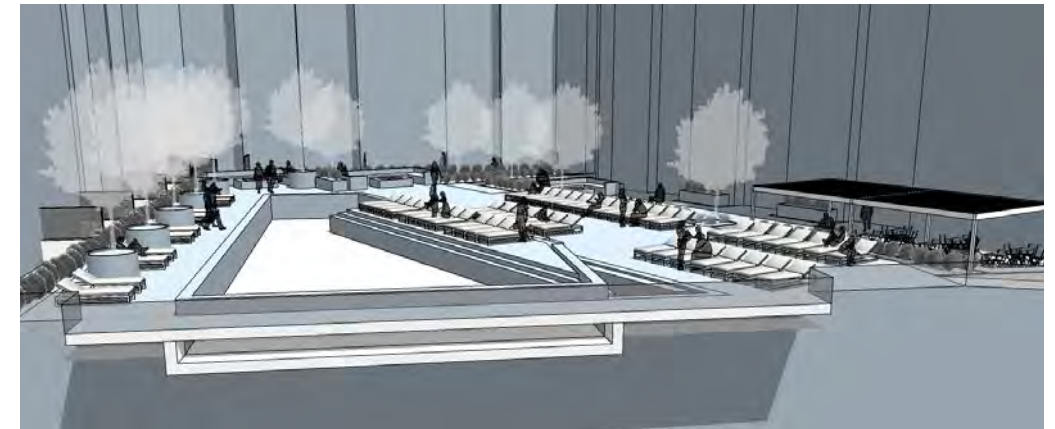
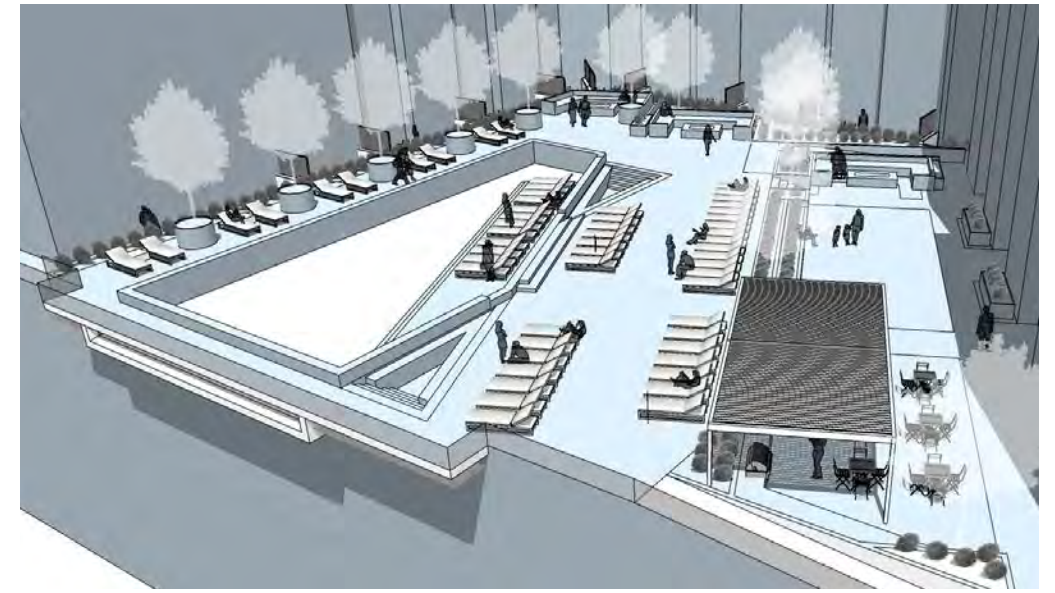
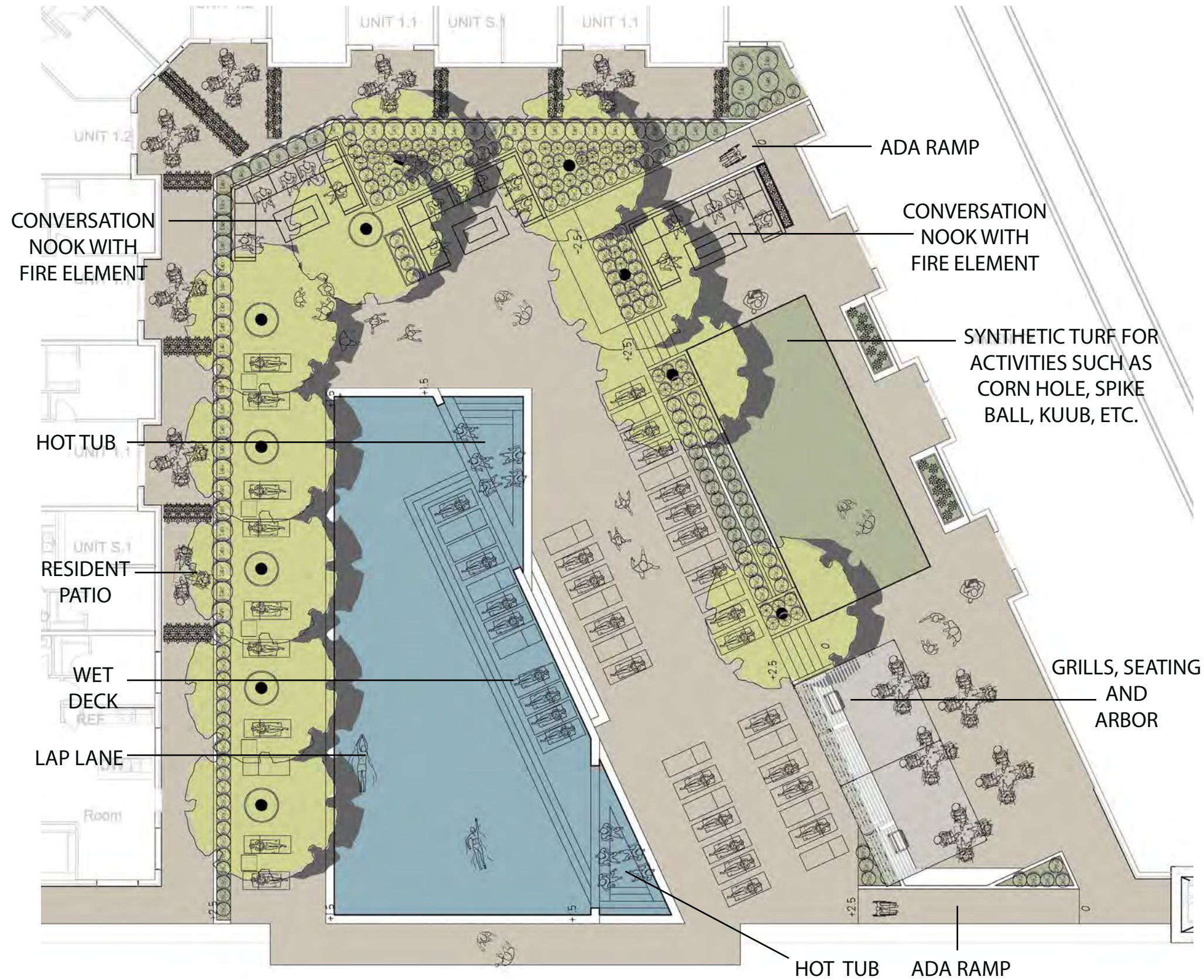


# LEVEL 04 EAST - LANDSCAPE PLAN | BUILDING LAYOUT





# LEVEL 04 WEST - LANDSCAPE PLAN | BUILDING LAYOUT









# LEVEL 06-10 - FLOOR PLAN | BUILDING LAYOUT



LEVEL 6-10 FLOOR PLAN



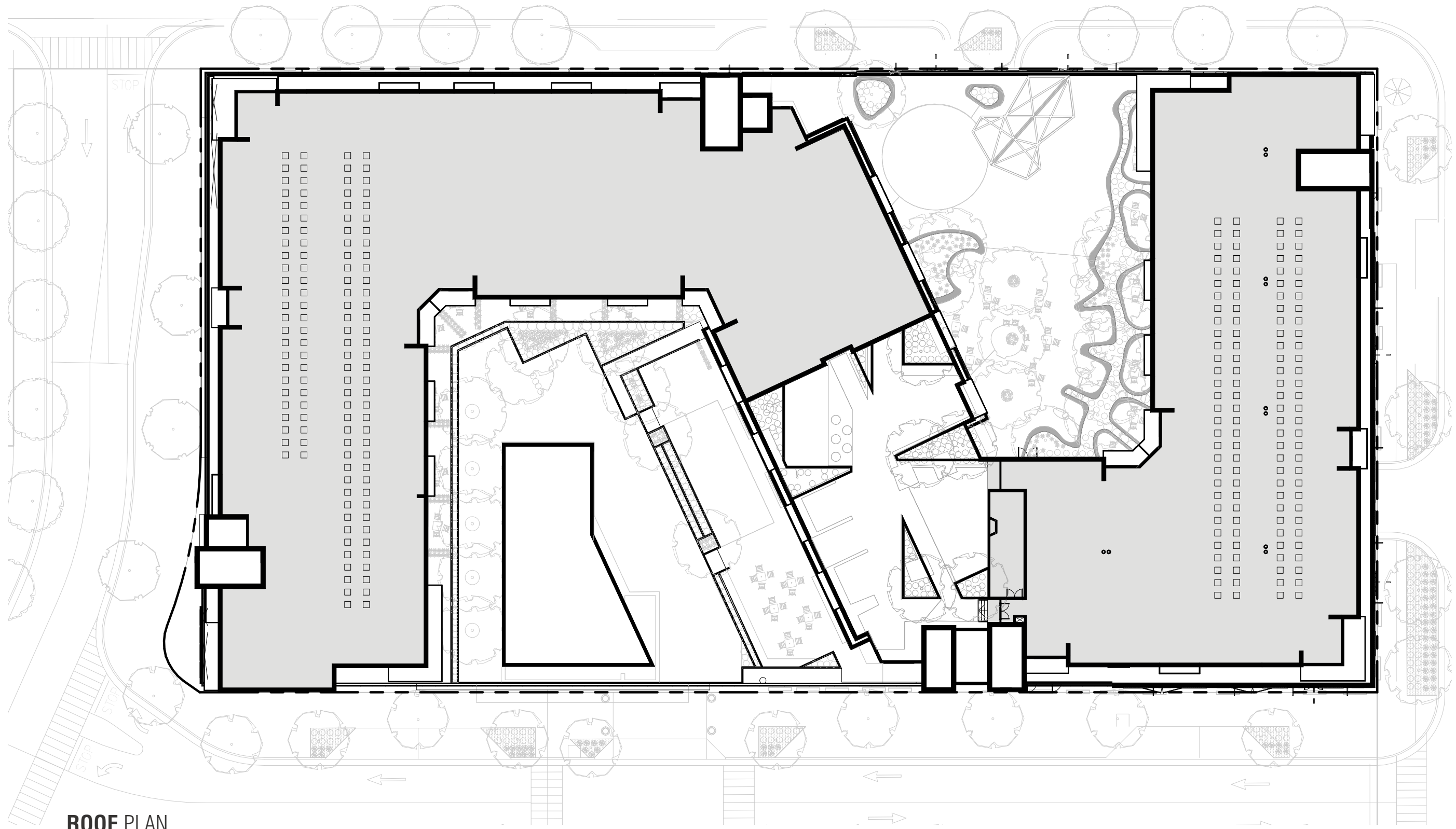
# LEVEL 11 - FLOOR PLAN | BUILDING LAYOUT



LEVEL 11 FLOOR PLAN







**ROOF PLAN**





# NORTHEAST PERSPECTIVE | EXTERIOR DESIGN





# NORTHWEST STREET CORNER | EXTERIOR DESIGN





NORTHWEST FACADE CORNER | **EXTERIOR DESIGN**





# SOUTHEAST PERSPECTIVE | EXTERIOR DESIGN





SOUTHEAST STREET CORNER | EXTERIOR DESIGN





# SOUTHEAST FACADE CORNER | EXTERIOR DESIGN





NORTHWEST PERSPECTIVE | EXTERIOR DESIGN





# WEST PERSPECTIVE | EXTERIOR DESIGN





# SOUTHWEST PERSPECTIVE | EXTERIOR DESIGN





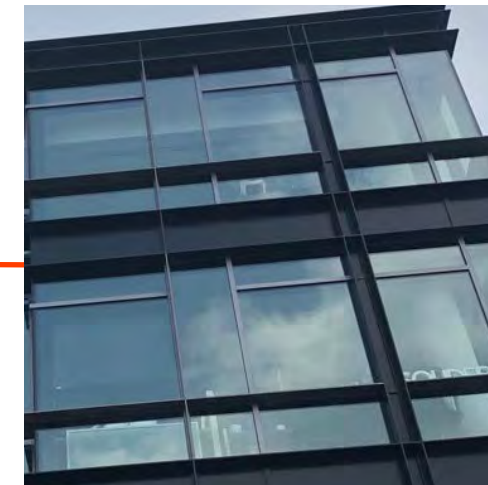
# MATERIAL PALETTE | EXTERIOR DESIGN



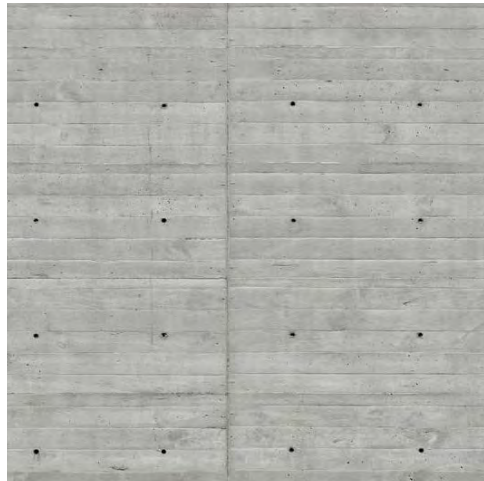
BRICK MASONRY WITH  
INSET METAL PANEL



CANTILEVERED BALCONY  
WITH STEEL HANDRAIL



DARK METAL PANEL  
W/ INTEGRAL REVEALS



EXPOSED CONCRETE  
STRUCTURAL CORE



METAL PARKING GARAGE  
SCREEN

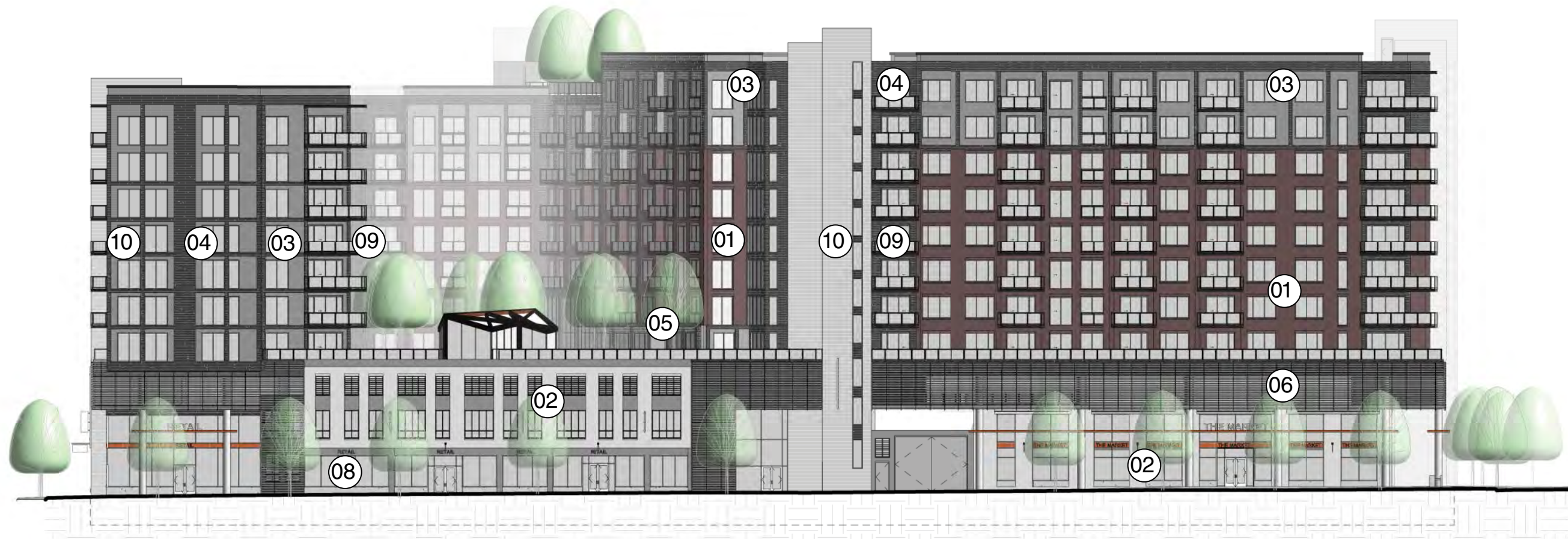


LIGHT MASONRY

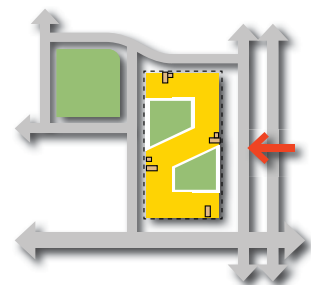


LIGHT MASONRY WITH  
STOREFRONT GLAZING





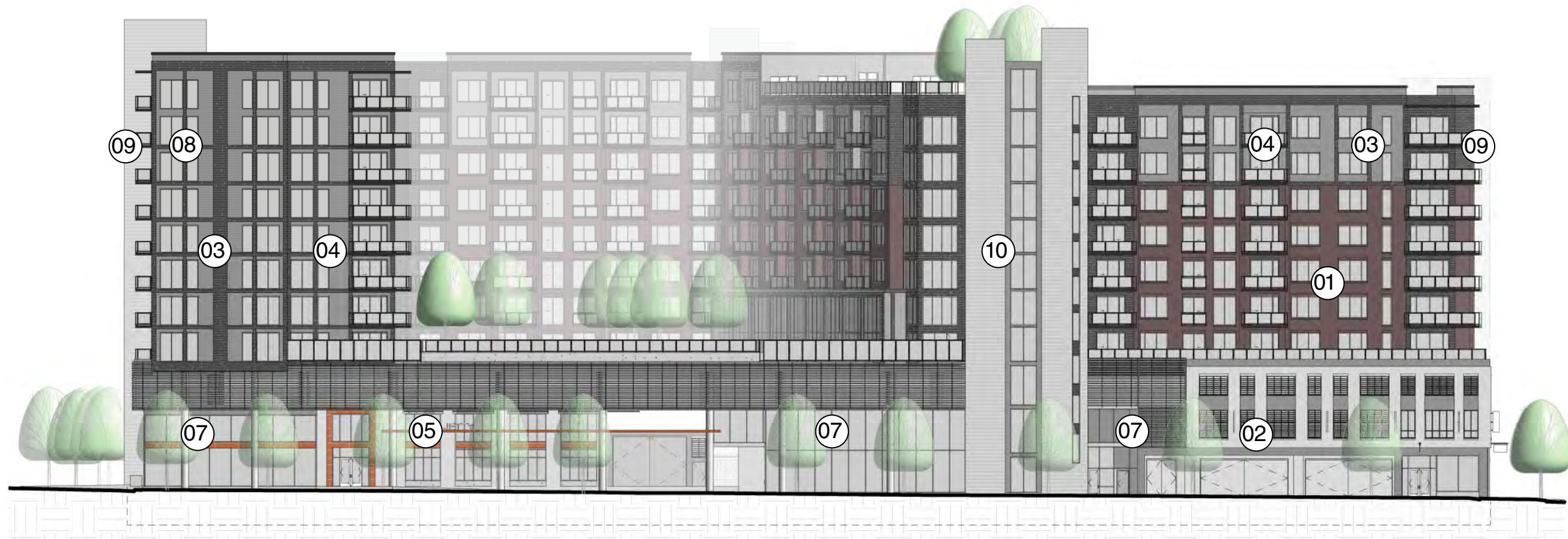
**EAST ELEVATION (STATE STREET)**



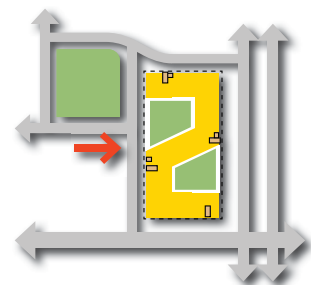
01) DARK MASONRY	04) LIGHT GRAY METAL PANEL	07) DARK ALUMINUM STOREFRONT	10) FORM-LINER CONCRETE STAIR CORES
02) LIGHT MASONRY	05) ACCENT COLOR METAL FASCIA	08) ALUMINUM WINDOW SYSTEM	
03) DARK GRAY METAL PANEL	06) ACCENT COLOR STEEL TUBE SCREEN	09) ALUMINUM HUNG BALCONIES AND JULIET BALCONIES	



# WEST ELEVATION | EXTERIOR DESIGN



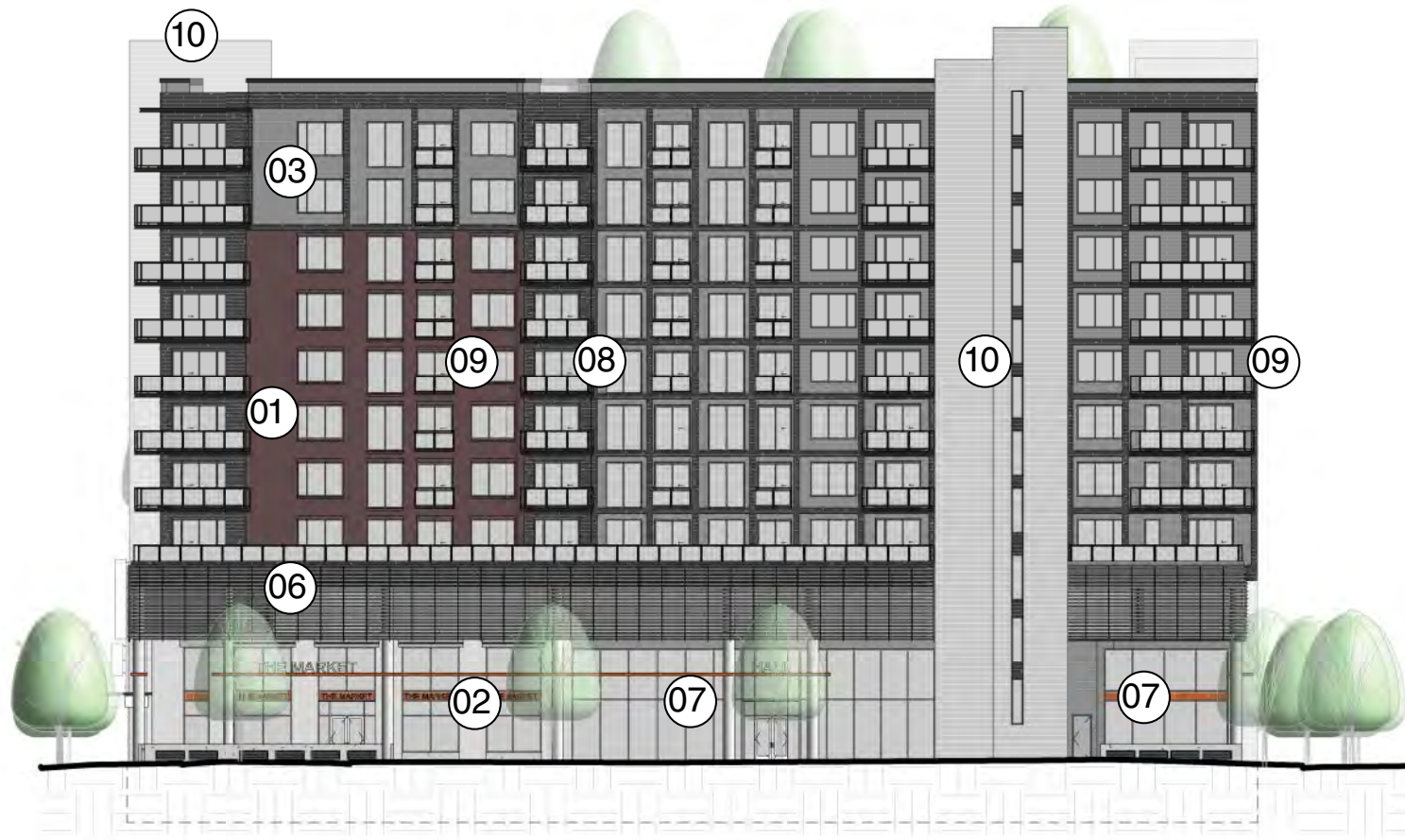
**WEST ELEVATION (MAJOR STREET)**



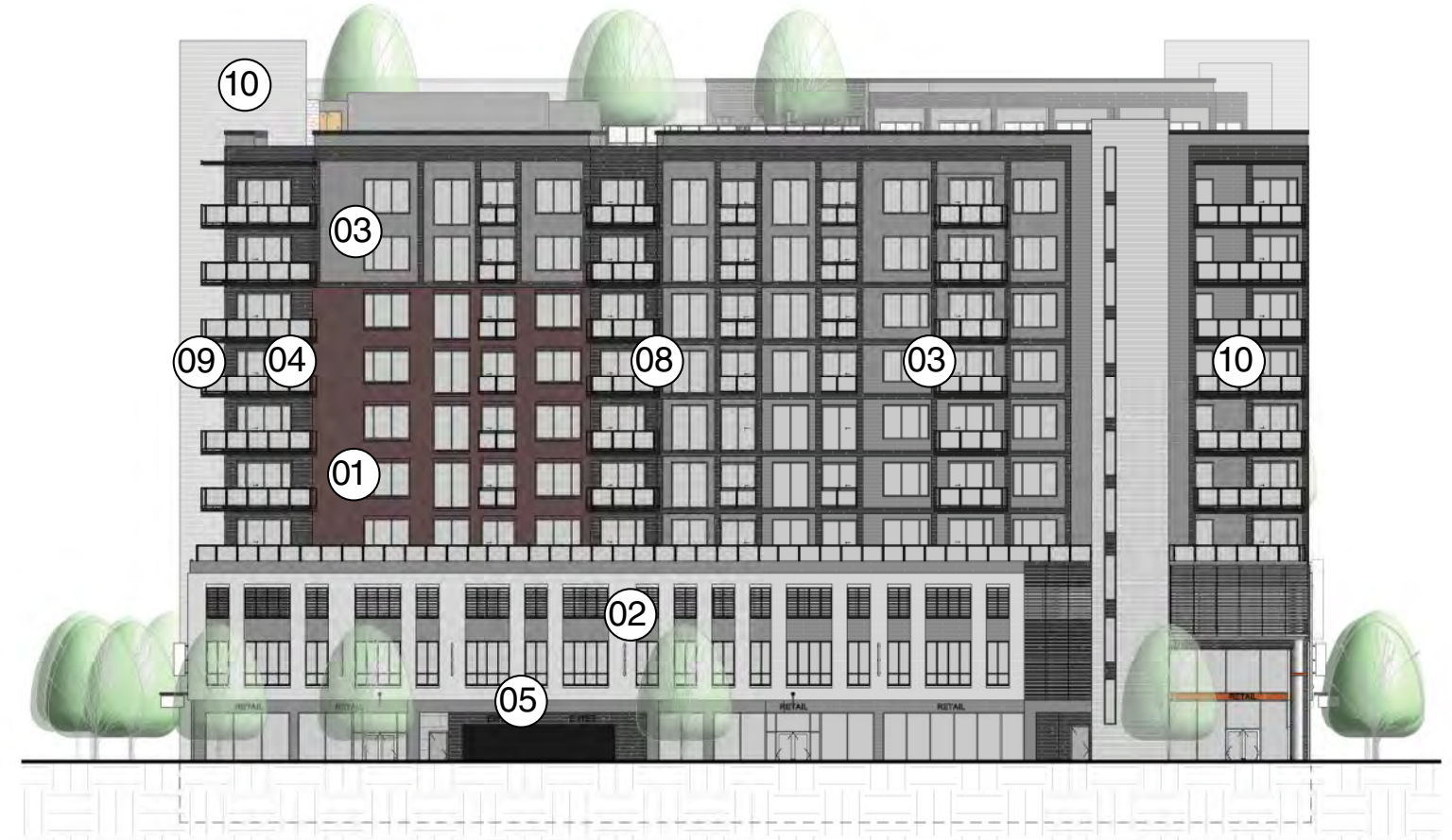
01) DARK MASONRY	04) LIGHT GRAY METAL PANEL	07) DARK ALUMINUM STOREFRONT	10) FORM-LINER CONCRETE STAIR CORES
02) LIGHT MASONRY	05) ACCENT COLOR METAL	08) ALUMINUM WINDOW SYSTEM	
03) DARK GRAY METAL PANEL	06) ACCENT COLOR STEEL TUBE SCREEN	09) ALUMINUM HUNG BALCONIES AND JULIET BALCONIES	



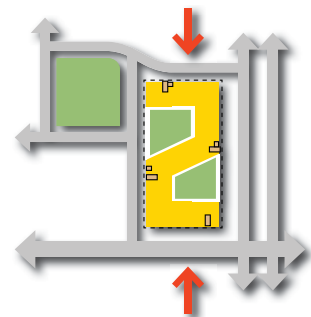
# NORTH & SOUTH ELEVATIONS | EXTERIOR DESIGN



**NORTH ELEVATION (LOWELL STREET)**



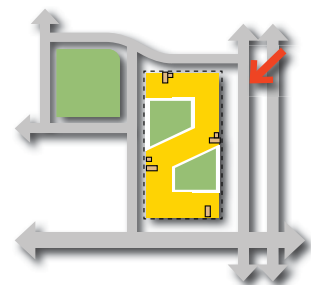
**SOUTH ELEVATION (800 SOUTH)**



01) DARK MASONRY	04) LIGHT GRAY METAL PANEL	07) DARK ALUMINUM STOREFRONT	10) FORM-LINER CONCRETE STAIR CORES
02) LIGHT MASONRY	05) ACCENT COLOR METAL FASCIA	08) ALUMINUM WINDOW SYSTEM	11) ROOFTOP MECHANICAL SCREEN
03) DARK GRAY METAL PANEL	06) ACCENT COLOR STEEL TUBE SCREEN	09) ALUMINUM HUNG BALCONIES AND JULIET BALCONIES	



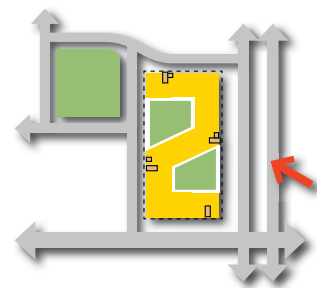
# PEDESTRIAN VIEW | USER EXPERIENCE



- 01 RETAIL STOREFRONT
- 02 PARKING GARAGE SCREEN
- 03 RESIDENTIAL TERRACE
- 04 SIGNAGE OPPORTUNITY
- 05 SERVICE DRIVE
- 06 OUTDOOR DINING



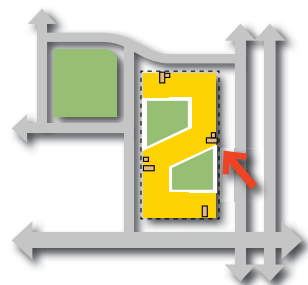
# PEDESTRIAN VIEW | USER EXPERIENCE



- 01 EAST RESIDENTIAL ENTRY
- 02 PARKING GARAGE SCREEN
- 03 RESIDENTIAL TERRACE
- 04 COMMERCIAL SPACE
- 05 SERVICE DRIVE
- 06 EXPOSED CONCRETE CORE



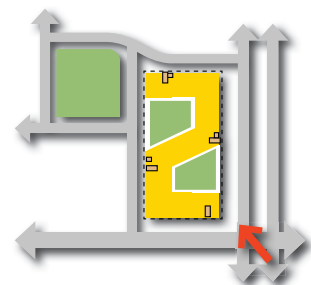
# PEDESTRIAN VIEW | USER EXPERIENCE



- ① EAST RESIDENTIAL ENTRY
- ② STREET TREES
- ③ PARKING GARAGE SCREEN
- ④ COMMERCIAL ENTRY RECESS
- ⑤ COMMERCIAL SPACE
- ⑥ SERVICE DRIVE
- ⑦ EXPOSED CONCRETE CORE
- ⑧ BIKE PARKING



# PEDESTRIAN VIEW | USER EXPERIENCE



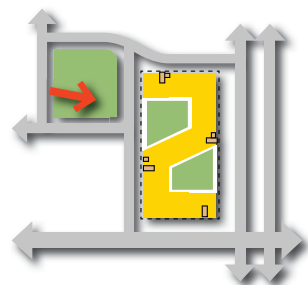
- |                          |                    |                         |
|--------------------------|--------------------|-------------------------|
| ① EAST RESIDENTIAL ENTRY | ④ COMMERCIAL ENTRY | ⑦ EXPOSED CONCRETE CORE |
| ② EAST AMENITY DECK      | ⑤ COMMERCIAL SPACE | ⑧ RESIDENTIAL TERRACE   |
| ③ PARKING GARAGE SCREEN  | ⑥ EGRESS CORRIDOR  |                         |



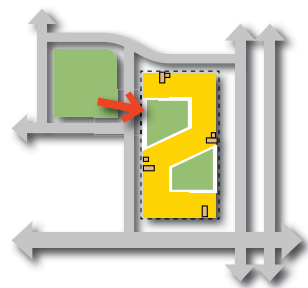
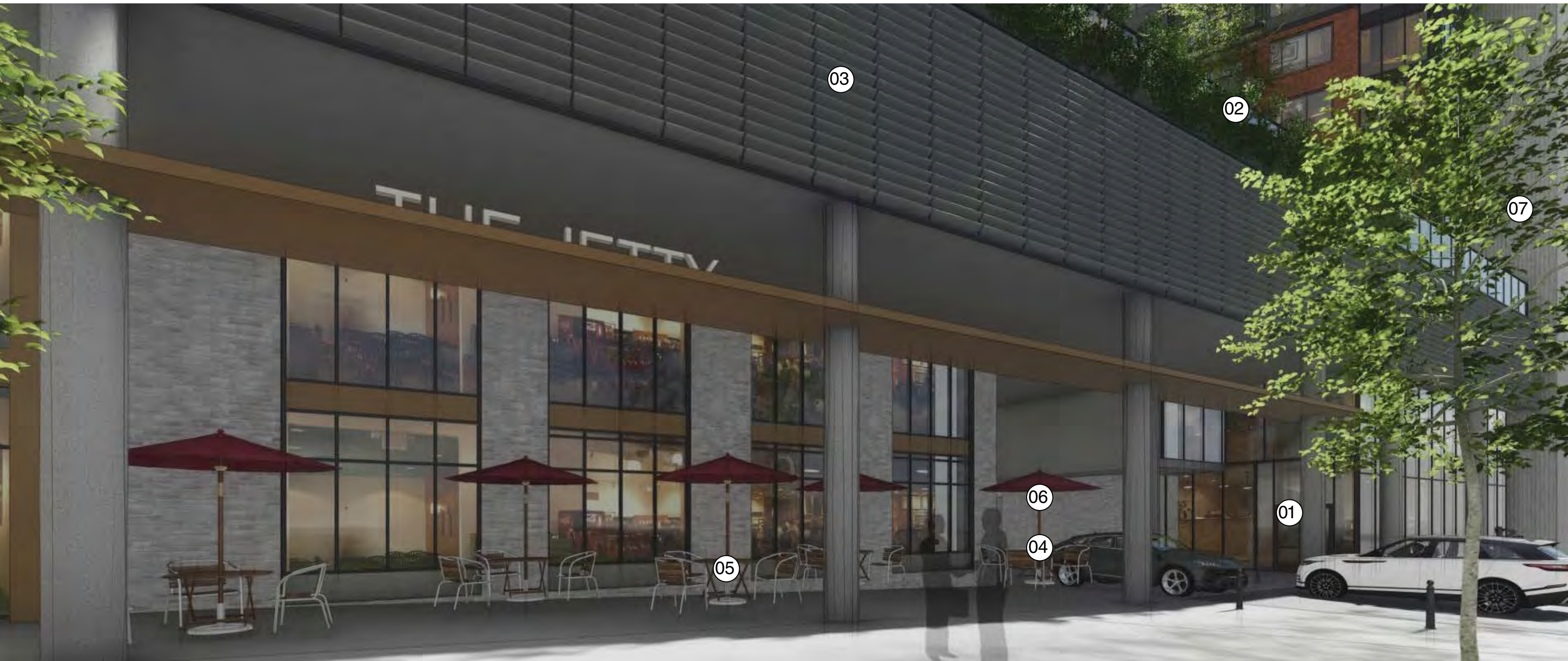
# PEDESTRIAN VIEW | USER EXPERIENCE



- |                           |                            |                          |
|---------------------------|----------------------------|--------------------------|
| 01 WEST RESIDENTIAL ENTRY | 04 LEASING LOBBY ENTRY     | 07 EXPOSED CONCRETE CORE |
| 02 WEST AMENITY DECK      | 05 11TH FLOOR ROOF AMENITY |                          |
| 03 PARKING GARAGE SCREEN  | 06 RESIDENTIAL TERRACE     |                          |







01 WEST RESIDENTIAL ENTRY

02 WEST AMENITY DECK

03 PARKING GARAGE SCREEN

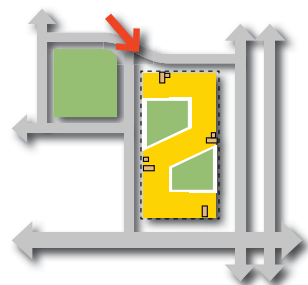
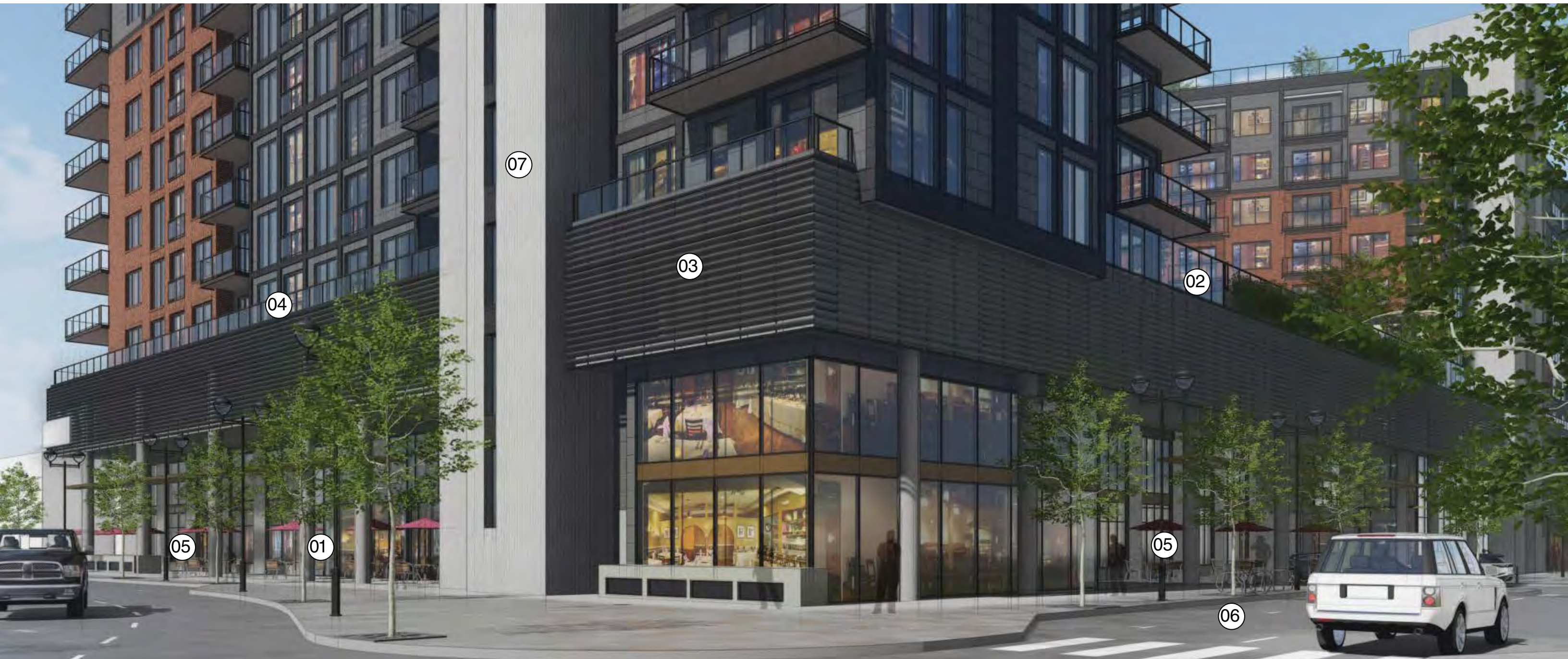
04 SERVICE DRIVE ENTRY

05 OUTDOOR DINING/SEATING

06 PARKING GARAGE ENTRY

07 EXPOSED CONCRETE CORE

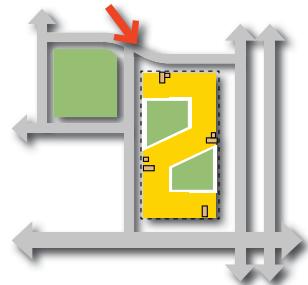
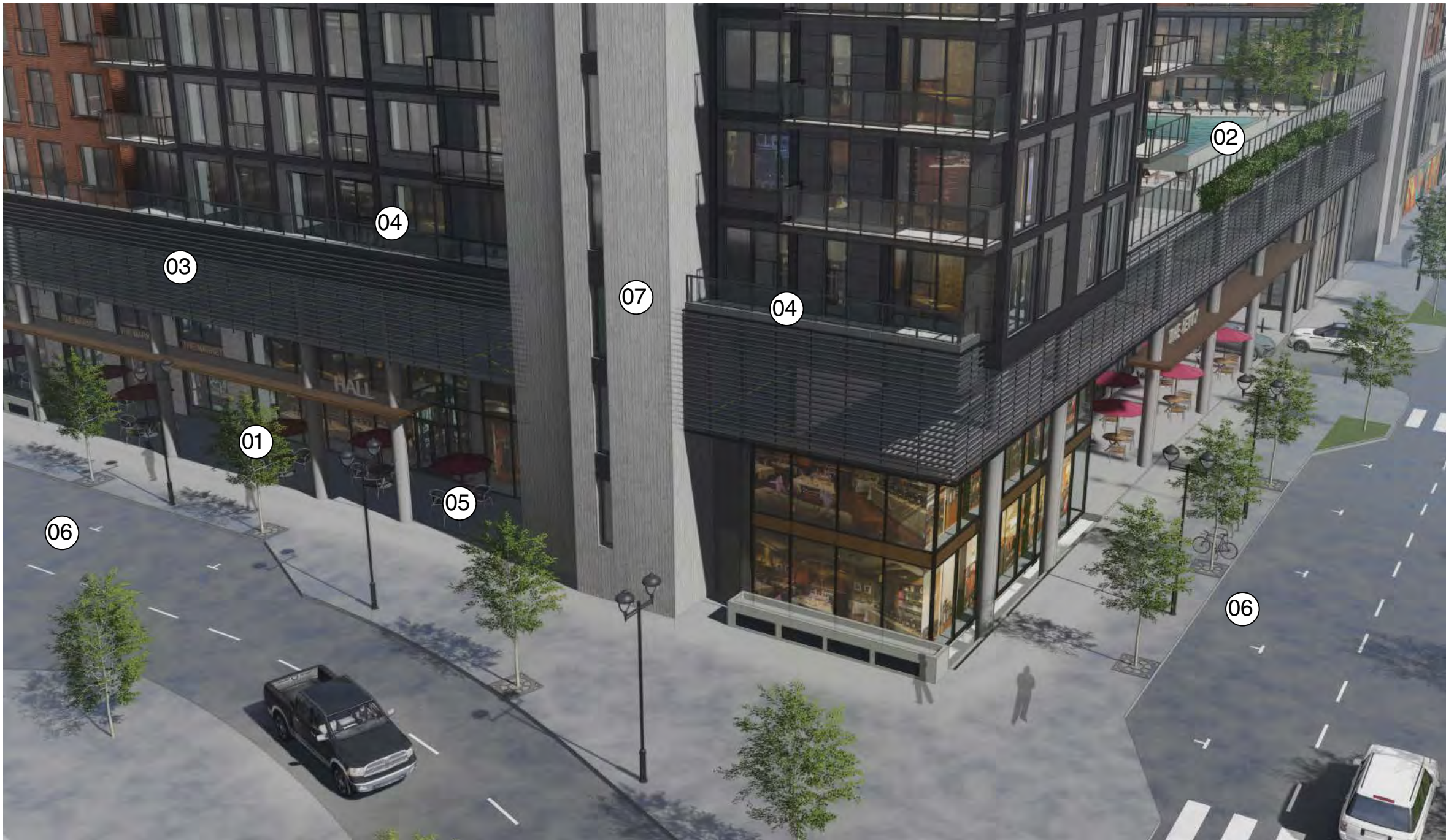




- |                          |                           |                          |
|--------------------------|---------------------------|--------------------------|
| 01 RETAIL ENTRY          | 04 RESIDENTIAL TERRACE    | 07 EXPOSED CONCRETE CORE |
| 02 WEST AMENITY DECK     | 05 OUTDOOR DINING/SEATING |                          |
| 03 PARKING GARAGE SCREEN | 06 PARALLEL PARKING       |                          |



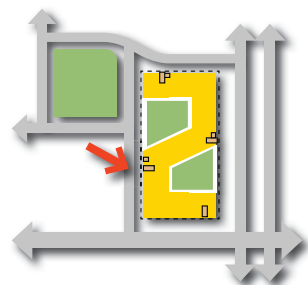
# PLINTH LEVEL VIEW | USER EXPERIENCE



- ① RETAIL ENTRY
- ② WEST AMENITY DECK
- ③ PARKING GARAGE SCREEN
- ④ RESIDENTIAL TERRACE
- ⑤ OUTDOOR DINING/SEATING
- ⑥ PARALLEL PARKING
- ⑦ EXPOSED CONCRETE CORE



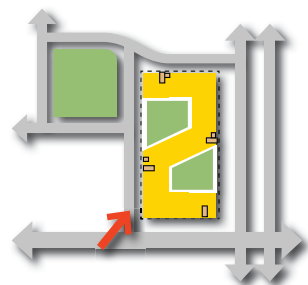
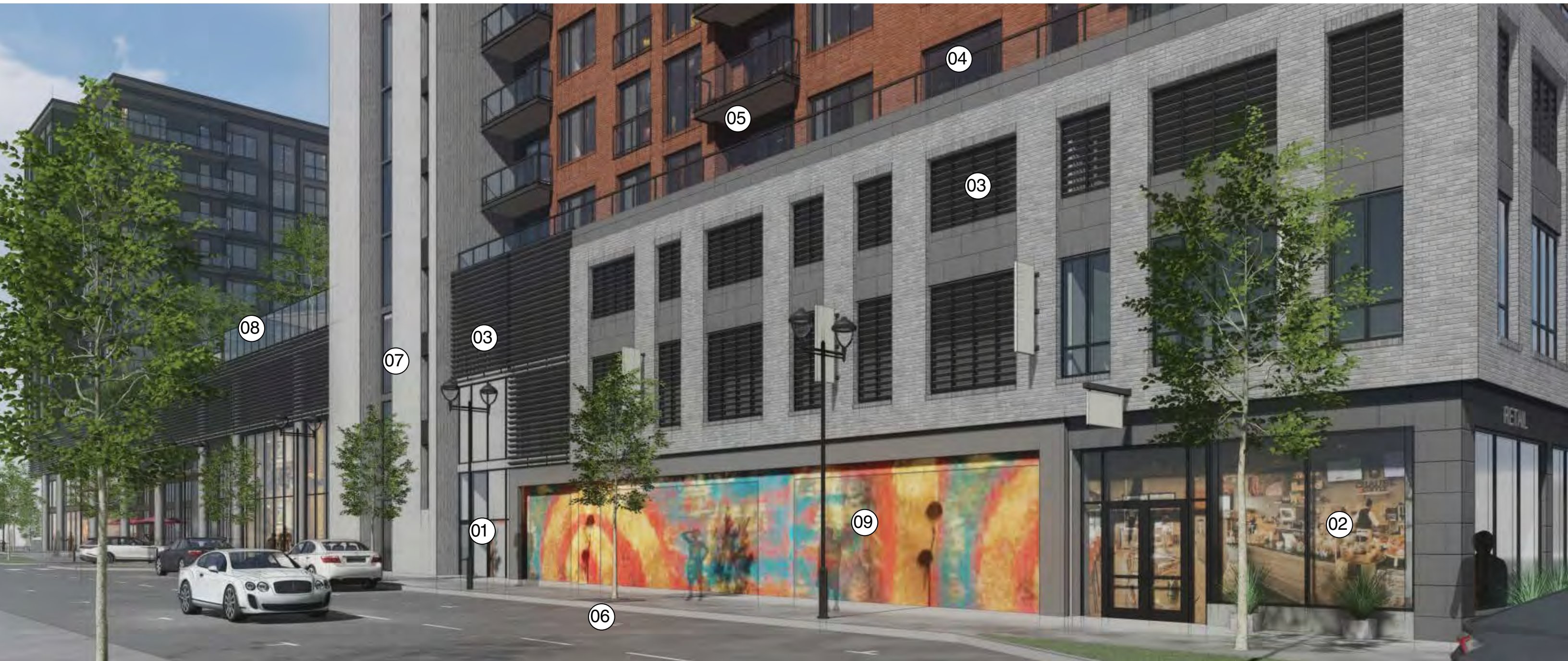
# PEDESTRIAN VIEW | USER EXPERIENCE



- 01 WEST RESIDENTIAL ENTRY
- 02 TRANSFORMERS
- 03 PARKING GARAGE SCREEN
- 04 RESIDENTIAL TERRACE
- 05 RESIDENTIAL BALCONY
- 06 PARALLEL PARKING
- 07 EXPOSED CONCRETE CORE



# PEDESTRIAN VIEW | USER EXPERIENCE



- |                          |                       |                         |
|--------------------------|-----------------------|-------------------------|
| ① WEST RESIDENTIAL ENTRY | ④ RESIDENTIAL TERRACE | ⑦ EXPOSED CONCRETE CORE |
| ② COMMERCIAL SPACE       | ⑤ RESIDENTIAL BALCONY | ⑧ WEST AMENITY DECK     |
| ③ PARKING GARAGE SCREEN  | ⑥ PARALLEL PARKING    | ⑨ TRANSFORMERS          |



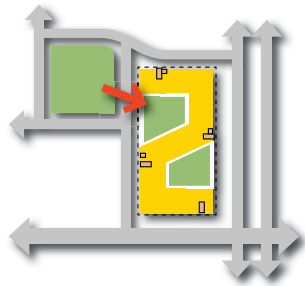
# PLINTH LEVEL VIEW | USER EXPERIENCE



- 01 LEASING LOBBY ENTRY
- 02 WEST AMENITY DECK
- 03 PARKING GARAGE SCREEN
- 04 RESIDENTIAL TERRACE
- 05 OUTDOOR DINING/SEATING
- 06 PARALLEL PARKING
- 07 EXPOSED CONCRETE CORE
- 08 RESIDENTIAL AMENITY

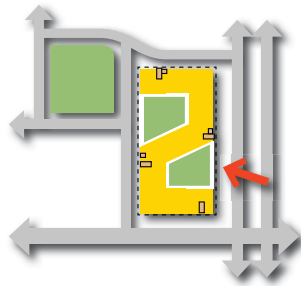
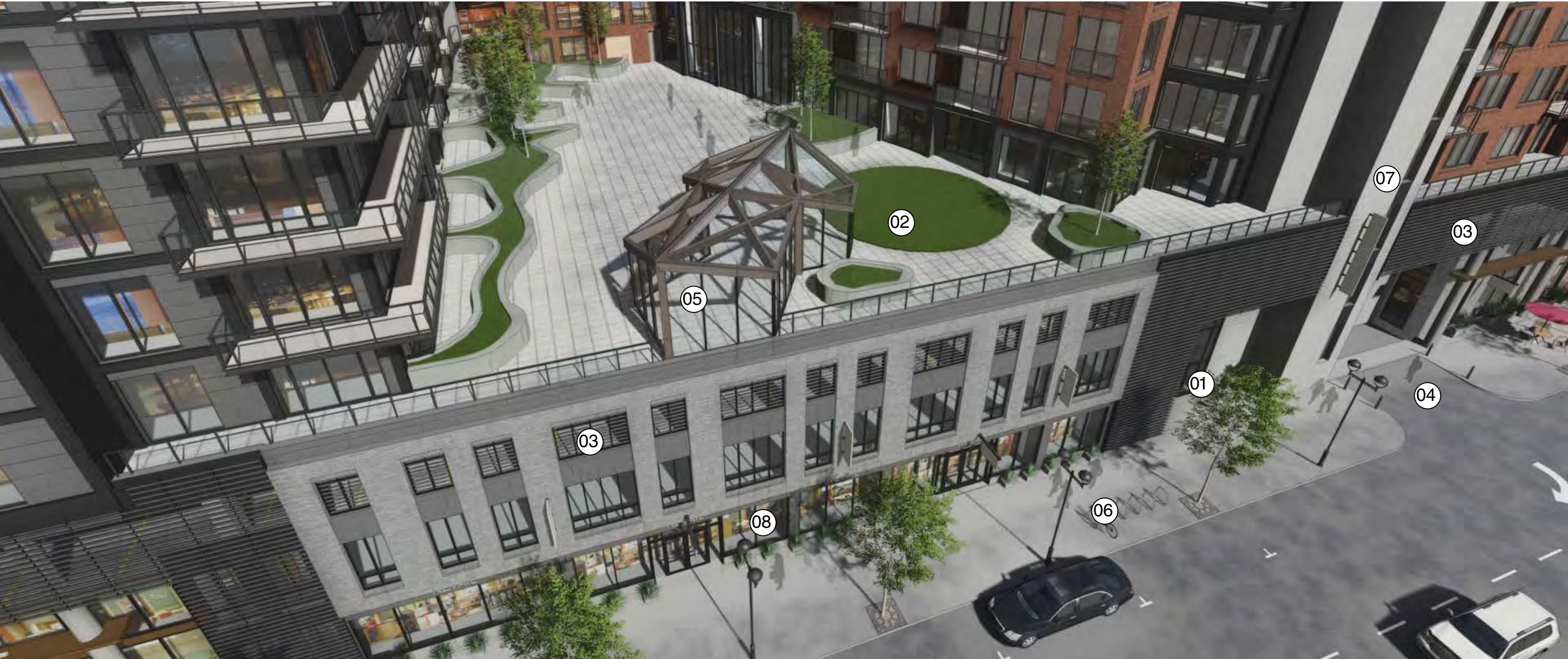


# PLINTH LEVEL VIEW | USER EXPERIENCE





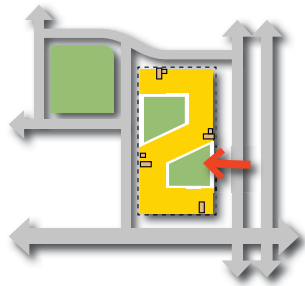
# PLINTH LEVEL VIEW | USER EXPERIENCE



- 01 EAST LOBBY ENTRY
- 02 EAST AMENITY DECK
- 03 PARKING GARAGE SCREEN
- 04 SERVICE DRIVE EXIT
- 05 NEST PAVILION
- 06 BIKE PARKING
- 07 EXPOSED CONCRETE CORE
- 08 COMMERCIAL SPACE



# PLINTH LEVEL VIEW | USER EXPERIENCE





SALT LAKE CITY PLANNING AND ZONING REQUIREMENTS:

21A.30.030: D-2 DOWNTOWN SUPPORT DISTRICT:

A. Purpose Statement: The purpose of the D-2 Downtown Support Commercial District is to provide an area that fosters the development of a sustainable urban neighborhood that accommodates commercial, office, residential and other uses that relate to and support the Central Business District. Development within the D-2 Downtown Support Commercial District is intended to be less intensive than that of the Central Business District, with high lot coverage and buildings placed close to the sidewalk. This district is appropriate in areas where supported by applicable master plans. Design standards are intended to promote pedestrian oriented development with a strong emphasis on a safe and attractive streetscape.

**- THE PROPOSED REDEVELOPMENT OF THE SEARS BLOCK WAS ENVISIONED FOLLOWING THE CITY'S GUIDELINES, STANDARDS AND STATEMENT OF PURPOSE.**

B. Uses: Uses in the D-2 Downtown Support District, as specified in section 21A.33.050, "Table Of Permitted And Conditional Uses For Downtown Districts", of this title, are permitted subject to the general provisions set forth in section 21A.30.010 of this chapter and this section.

**- THE COMBINATION OF USES FOR THE REDEVELOPMENT OF THE SEARS BLOCK WAS CRAFTED WITH THE INTENT OF CREATING COMMUNITY WITH A STRONG SENSE OF PLACE.**

C. Lot Size Requirements: No minimum lot area or lot width shall be required.

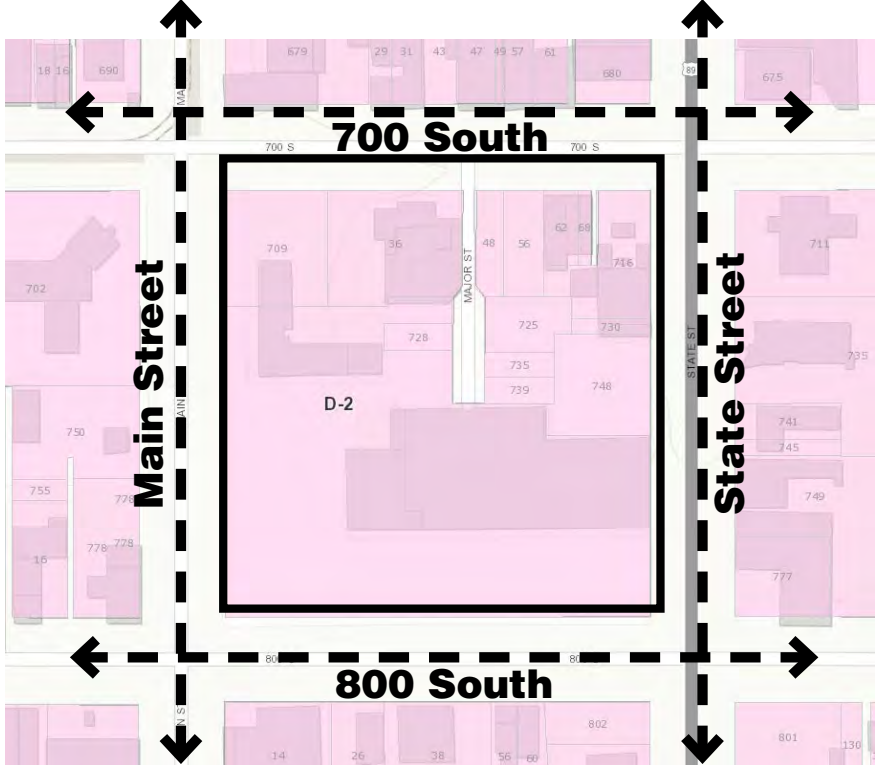
**- THE PROPOSED REDEVELOPMENT OF THE SEARS BUILDING IS BASED ON THE FINE-GRAIN REFINEMENT THAT LOOKS AT THE OVERALL SEARS BLOCK AND INTRODUCES A SERIES OF INTERNAL STREETS CREATING A PARCEL OF APPROXIMATELY 200'X400'.**

D. Maximum Building Height: The maximum permitted building height shall not exceed one hundred twenty feet (120') subject to the following review process: Buildings over sixty five

feet (65') in height are subject to design review according to the requirements of chapter 21A.59 of this title.

**- THE PROPOSED MIXED USE PROJECT INTRODUCES PARTS OF THE MASS AT 4 STORIES IN HEIGHT, LOWER THAN 65' ALLOWED. HOWEVER, WITH THE INTENT OF CREATING INTEREST TO THE URBAN SCAPE AND FACILITATE THE OF CREATING A TRULY PEDESTRIAN FOCUSED BUILDING THAT HIDES THE AUTOMOBILE AND THE SERVICE FUNCTIONS, THE BUILDING MASS INCORPORATES BUILDING VOLUMES THAT EXTEND TO THE MAXIMUM ALLOWABLE 120' BUILDING HEIGHT.**

E. Minimum Yard Requirements:  
1. Front And Corner Side Yard: There is no minimum setback. The maximum setback is ten feet (10').  
2. Interior Side Yards: No minimum side yard is required except a minimum of fifteen feet (15') side yard is required when the side yard is adjacent to a single or two family residential zoning district.  
3. Rear Yard: No minimum rear yard is required except a minimum of twenty five feet (25') rear yard is required when the rear yard is adjacent to a single or two family residential district.





4. Buffer Yards: Any lot abutting a lot in a residential district shall conform to the buffer yard requirements of Chapter 21A.48 of this title.

**- THE PROPOSED BUILDING GOES TO THE BUILD-TO-LINE AT THE PROPERTY LINE. THE NORTH FAÇADE, FACING SAPA, AND THE WEST FAÇADE FACING A FUTURE CENTRAL PARK ARE RECESSED ON THE GROUND FLOOR TO ALLOW A PROTECTED COLONNADE WITH STOREFRONT TO INCREASE THE TRANSPARENCY AND POROSITY OF THE PEDESTRIAN SIDEWALK EXPERIENCE. THE LIVE-WORK UNITS ARE CONNECTED WITH THE STREET ROW WITH SMALL ENGAGING PORCHES.**

F. Landscape Yard Requirements: If a front or corner side yard is provided, such yard shall be maintained as a landscaped yard. The landscaped yard can take the form of outdoor dining, patio, courtyard or plaza, subject to site plan review approval.

**- INDEED, THE VISION OF HARD/SOFT LANDSCAPING ON THE GROUND LEVEL AROUND THE DIFFERENCE USES WITH WHICH THE BUILDING CONNECTS WITH**

**THE SIDEWALK AND STREETS AROUND THE BLOCK ARE CAREFULLY CHOREOGRAPHED TO INCLUDE STREET LIGHTING (THAT MEETS CITY STANDARDS) AS WELL AS WAYFINDING THE FACILITATES THE NAVIGATION AROUND THE NEIGHBORHOOD AND OUTDOOR DINING SPACE.**

G. Parking Lot Setbacks: If a front or corner side yard is provided surface parking is prohibited in those areas. Surface parking lots that are not located completely behind the primary structure shall maintain a twenty foot (20') landscaped yard from the front and corner side yard property lines.

**- A COMPLETELY HIDDEN PARKING STRUCTURE IS PROPOSED WITH TWO ACCESS POINTS PLANNED ON THE SOUTH AND WEST SIDES OF THE BUILDING. PARKING FOR COMMERCIAL USES WILL BE ON THE GROUND LEVEL SURROUNDED BY LIVE-WORK UNITS FLANKING THE SIDEWALKS. THE COMMERCIAL PARKING GARAGE IS CONNECTED WITH PEDESTRIAN WALKS TO SURROUNDING STREETS TO SERVE THE GUEST/COSTUMER OF COMMERCIAL USES, AND THE LIVE-WORK FUNCTIONS. RESIDENTIAL PARKING WILL BE**

**LOCATED ON A BASEMENT LEVEL AS WELL AS TWO LEVELS ABOVE THE GROUND FLOOR.**

H. Mid-Block Walkways: Any new development shall provide a mid-block walkway if a mid-block walkway on the subject property has been identified in a master plan that has been adopted by the city. The following standards apply to the mid-block walkway:

1. The mid-block walkway must be a minimum of ten feet (10') wide and include a minimum six foot (6') wide unobstructed path.

2. The mid-block walkway may be incorporated into the building provided it is open to the public. A sign shall be posted indicating that the public may use the walkway.

**- THE OVERALL PLAN FOR THE SEARS BLOCK INTRODUCES MID-BLOCK STREET WITH AMPLE SIDEWALKS CONNECTING THE PERIMETER STREETS TO A CENTRAL PARK IN THE MIDDLE OF THE BLOCK.**

I. Ground Floor Uses: To activate the ground floor of structures, retail goods establishments, retail service establishments, public service portions of businesses, restaurants, taverns/brewpubs, bar

establishments, art galleries, theaters or performing art facilities are required on the ground floor of structures facing State Street, Main Street, 800 South and 900 South.

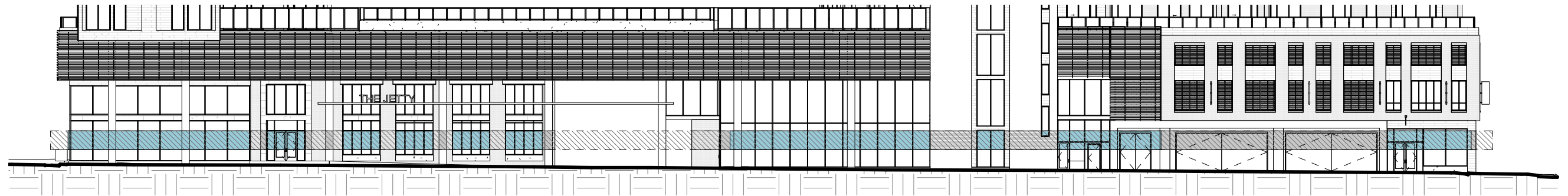
**- WITH THIS FIRST PHASE OF DEVELOPMENT OF THE SEARS BLOCK, THE GROUND LEVEL HAS BEEN CRAFTED TO MEET THE INTENT OF THE CODE BY OPTIMIZING A BLEND OF USES, OUTLINED IN THE CODE, TO ENSURE PEDESTRIAN ACTIVITY, CONNECTIVITY AND URBAN ENERGY IS THIS PART OF TOWN.**



# GLAZING COMPLIANCE DIAGRAM | CODE & ZONING

21A.37.050.C1 GROUND FLOOR GLASS

WALL AREA: 1,680 SF  
GLASS AREA: 1,046 SF  
PERCENTAGE:  
**62.26%**  
40% MIN REQ'D



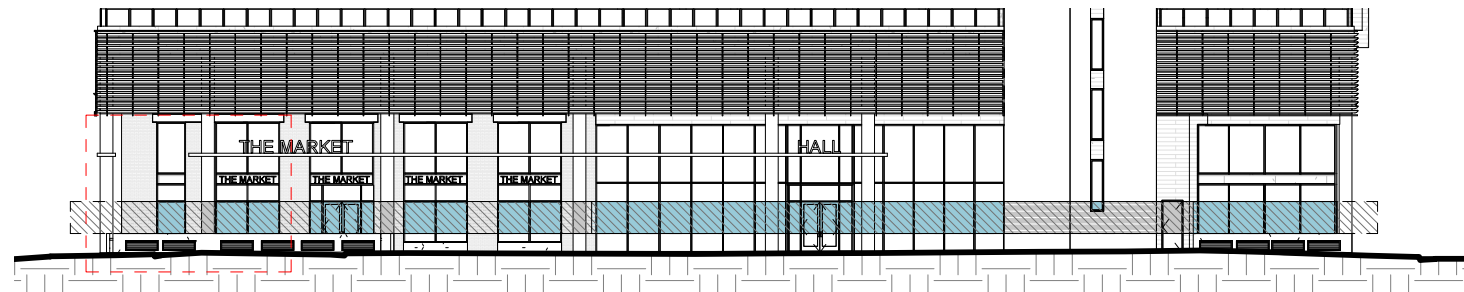
WEST ELEVATION (MAJOR STREET)

WALL AREA: 965 SF  
GLASS AREA: 586 SF  
PERCENTAGE:  
**60.73%**  
40% MIN REQ'D

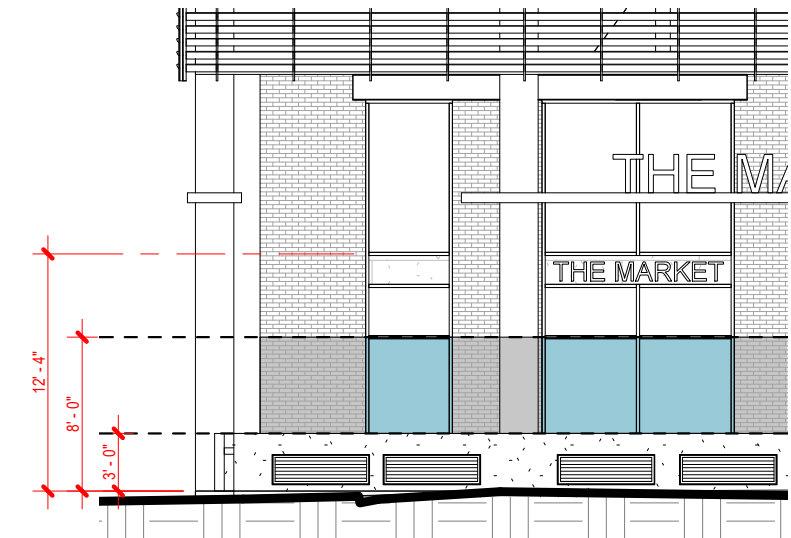


SOUTH ELEVATION (800 SOUTH)

WALL AREA: 948 SF  
GLASS AREA: 660 SF  
PERCENTAGE:  
**69.62%**  
40% MIN REQ'D



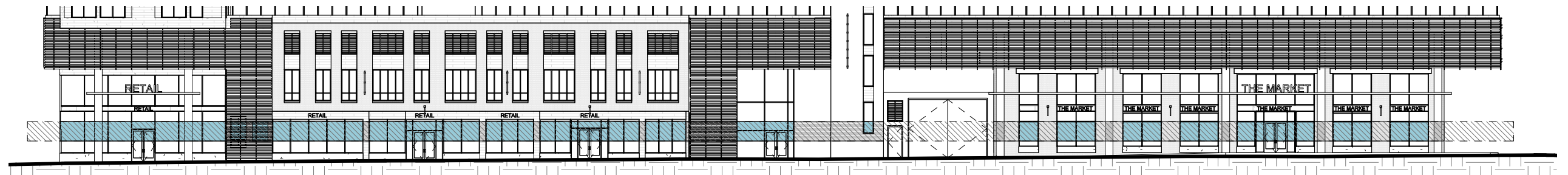
NORTH ELEVATION (LOWEL STREET)



SPECIFIED PERCENTAGE RANGE (3' TO 8') OF THE GROUND FLOOR

ENLARGED PARTIAL ELEVATION

WALL AREA: 1,637 SF  
GLASS AREA: 1,212 SF  
PERCENTAGE:  
**74.04%**  
40% MIN REQ'D



EAST ELEVATION (STATE STREET)



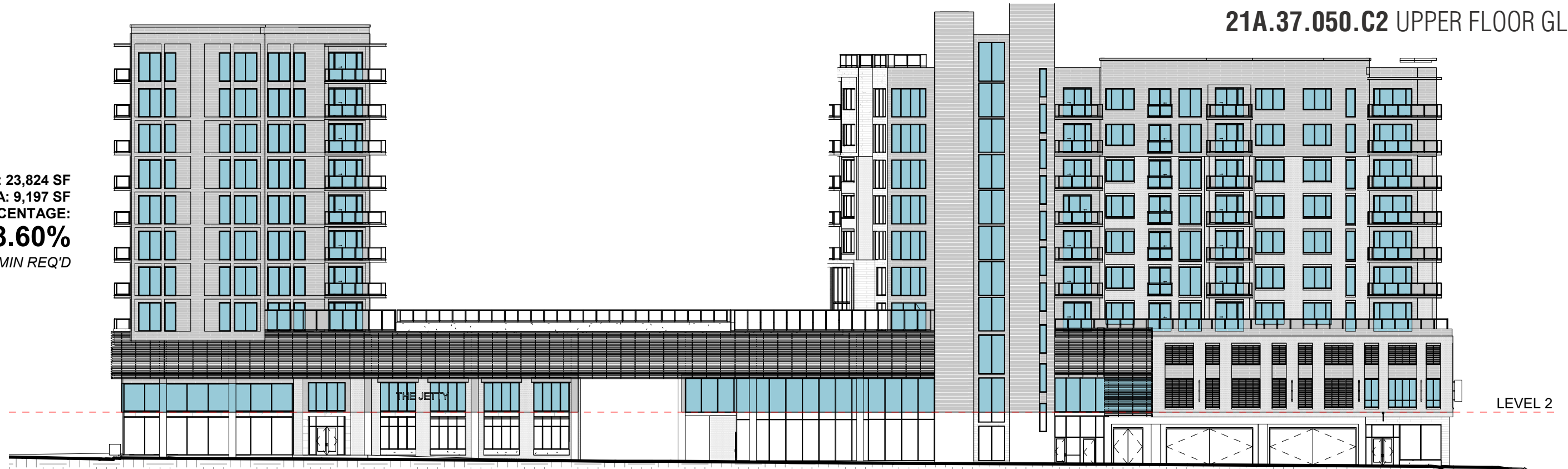
GLASS AREA WALL AREA 70



# GLAZING COMPLIANCE DIAGRAM | CODE & ZONING

21A.37.050.C2 UPPER FLOOR GLASS

WALL AREA: 23,824 SF  
GLASS AREA: 9,197 SF  
PERCENTAGE:  
**38.60%**  
25% MIN REQ'D



WEST ELEVATION - UPPER FLOORS (MAJOR STREET)

WALL AREA: 26,799 SF  
GLASS AREA: 10,015 SF  
PERCENTAGE:  
**37.37%**  
25% MIN REQ'D



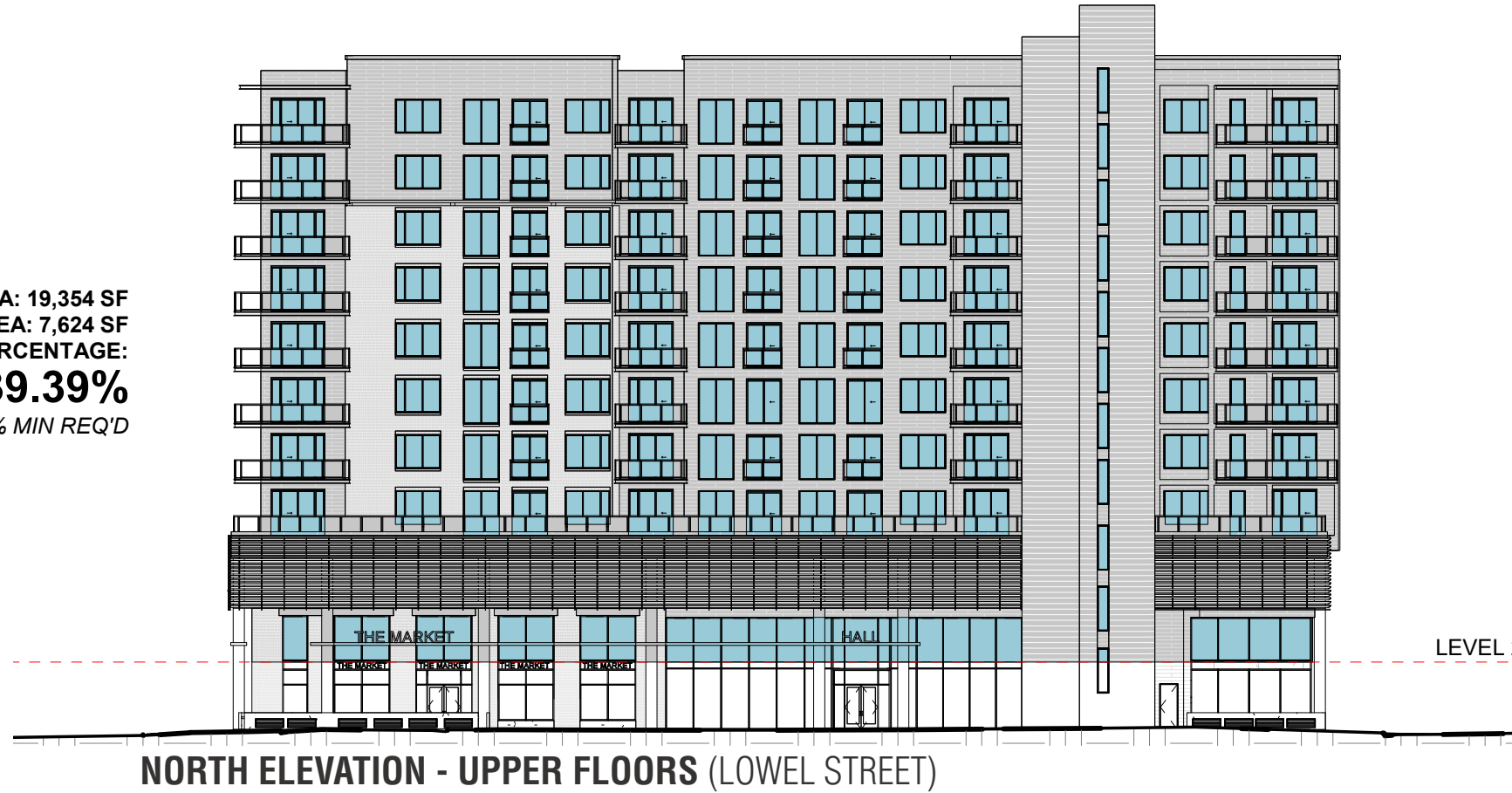
EAST ELEVATION - UPPER FLOORS (STATE STREET)



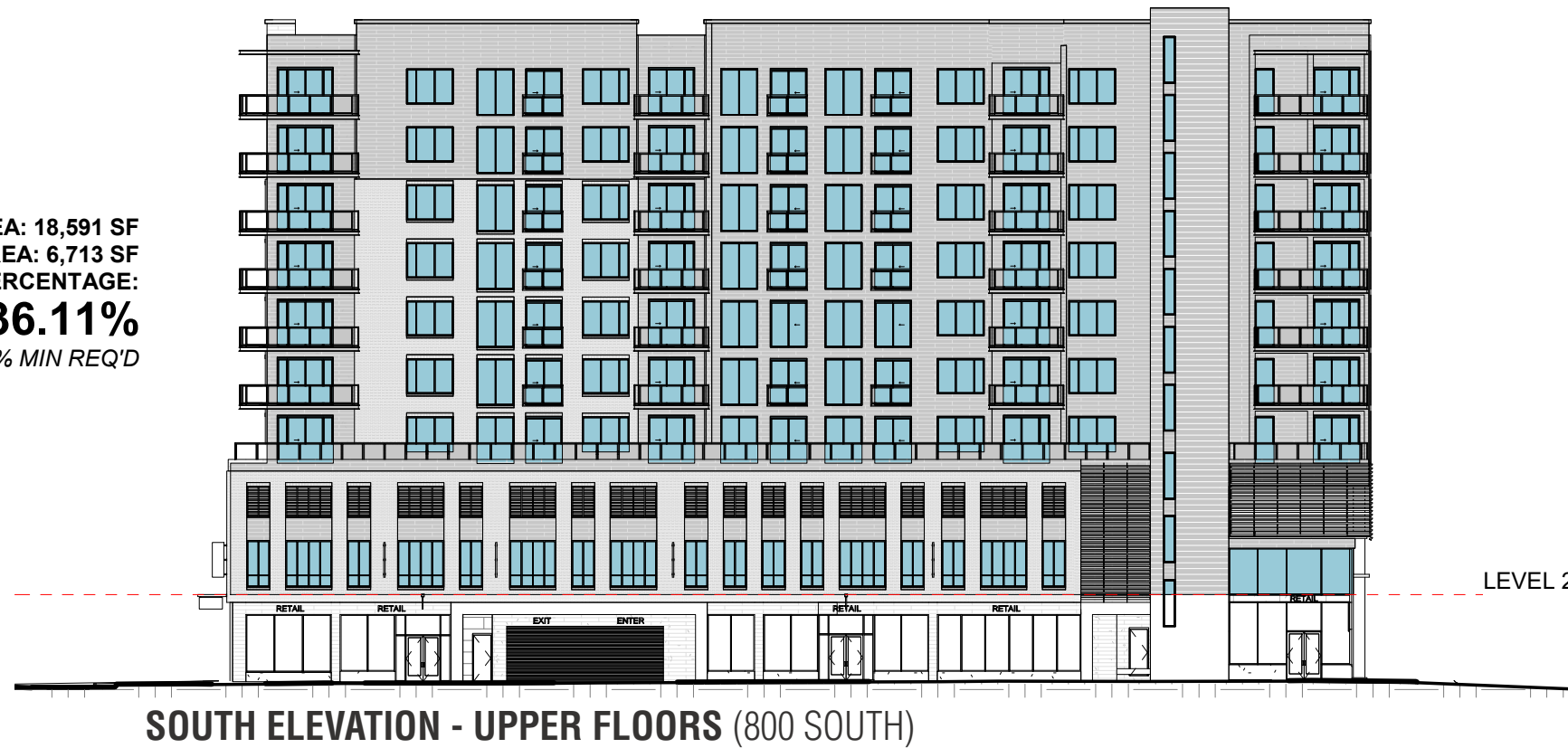
# GLAZING COMPLIANCE DIAGRAM | CODE & ZONING

21A.37.050.C2 UPPER FLOOR GLASS

WALL AREA: 19,354 SF  
GLASS AREA: 7,624 SF  
PERCENTAGE:  
**39.39%**  
25% MIN REQ'D



WALL AREA: 18,591 SF  
GLASS AREA: 6,713 SF  
PERCENTAGE:  
**36.11%**  
25% MIN REQ'D





# GLAZING COMPLIANCE DIAGRAM | CODE & ZONING

21A.37.050.C2 UPPER FLOOR GLASS



**A - SOUTH ELEVATION** (NORTH COURTYARD)

WALL AREA: 9,660 SF  
 GLASS AREA: 4,320 SF  
 PERCENTAGE:  
**44.72%**  
 25% MIN REQ'D



**B - SOUTHWEST ELEVATION** (NORTH COURTYARD)

WALL AREA: 751 SF  
 GLASS AREA: 416 SF  
 PERCENTAGE:  
**55.39%**  
 25% MIN REQ'D



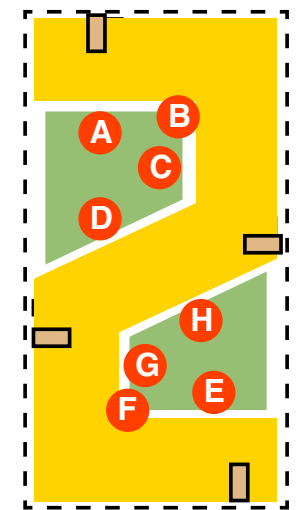
**C - WEST ELEVATION** (NORTH COURTYARD)

WALL AREA: 7,262 SF  
 GLASS AREA: 3,909 SF  
 PERCENTAGE:  
**53.83%**  
 25% MIN REQ'D



**D - NORTHWEST ELEVATION** (NORTH COURTYARD)

WALL AREA: 10,137 SF  
 GLASS AREA: 5,294 SF  
 PERCENTAGE:  
**52.22%**  
 25% MIN REQ'D

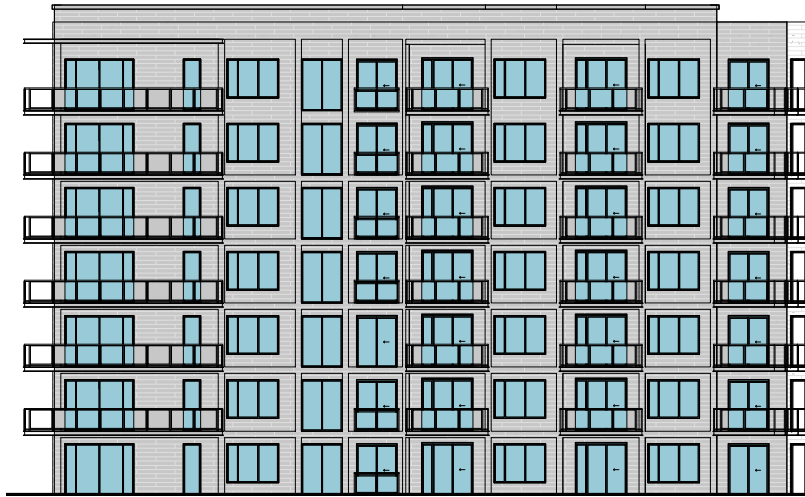


GLASS AREA WALL AREA 73



# GLAZING COMPLIANCE DIAGRAM | CODE & ZONING

21A.37.050.C2 UPPER FLOOR GLASS



**E - NORTH ELEVATION** (SOUTH COURTYARD)

WALL AREA: 8,645 SF  
 GLASS AREA: 3,782 SF  
 PERCENTAGE:  
**43.75%**  
 25% MIN REQ'D



**F - NORTHWEST ELEVATION** (SOUTH COURTYARD)

WALL AREA: 667 SF  
 GLASS AREA: 364 SF  
 PERCENTAGE:  
**54.57%**  
 25% MIN REQ'D



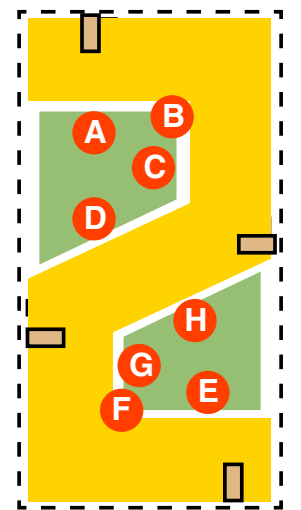
**G - EAST ELEVATION** (SOUTH COURTYARD)

WALL AREA: 3,806 SF  
 GLASS AREA: 1,686 SF  
 PERCENTAGE:  
**44.30%**  
 25% MIN REQ'D



**H - SOUTHEAST ELEVATION** (SOUTH COURTYARD)

WALL AREA: 9,422 SF  
 GLASS AREA: 4,624 SF  
 PERCENTAGE:  
**49.08%**  
 25% MIN REQ'D

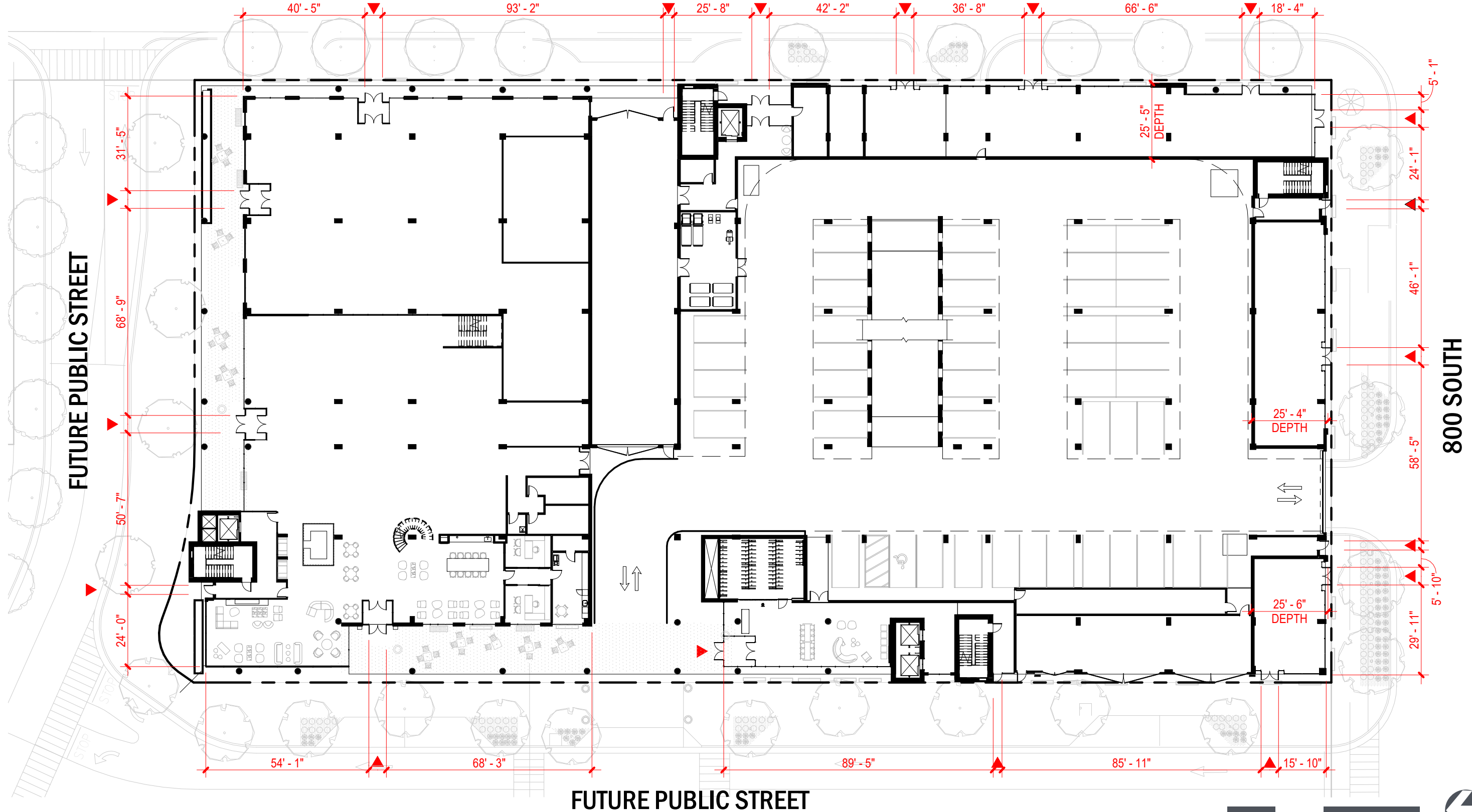




# LEVEL 01 - BUILDING ENTRIES | CODE & ZONING

21A.37.050.D BUILDING ENTRIES

STATE STREET



800 SOUTH

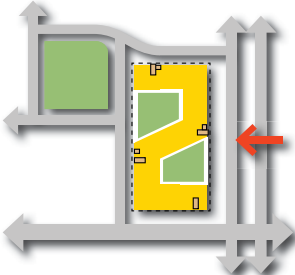
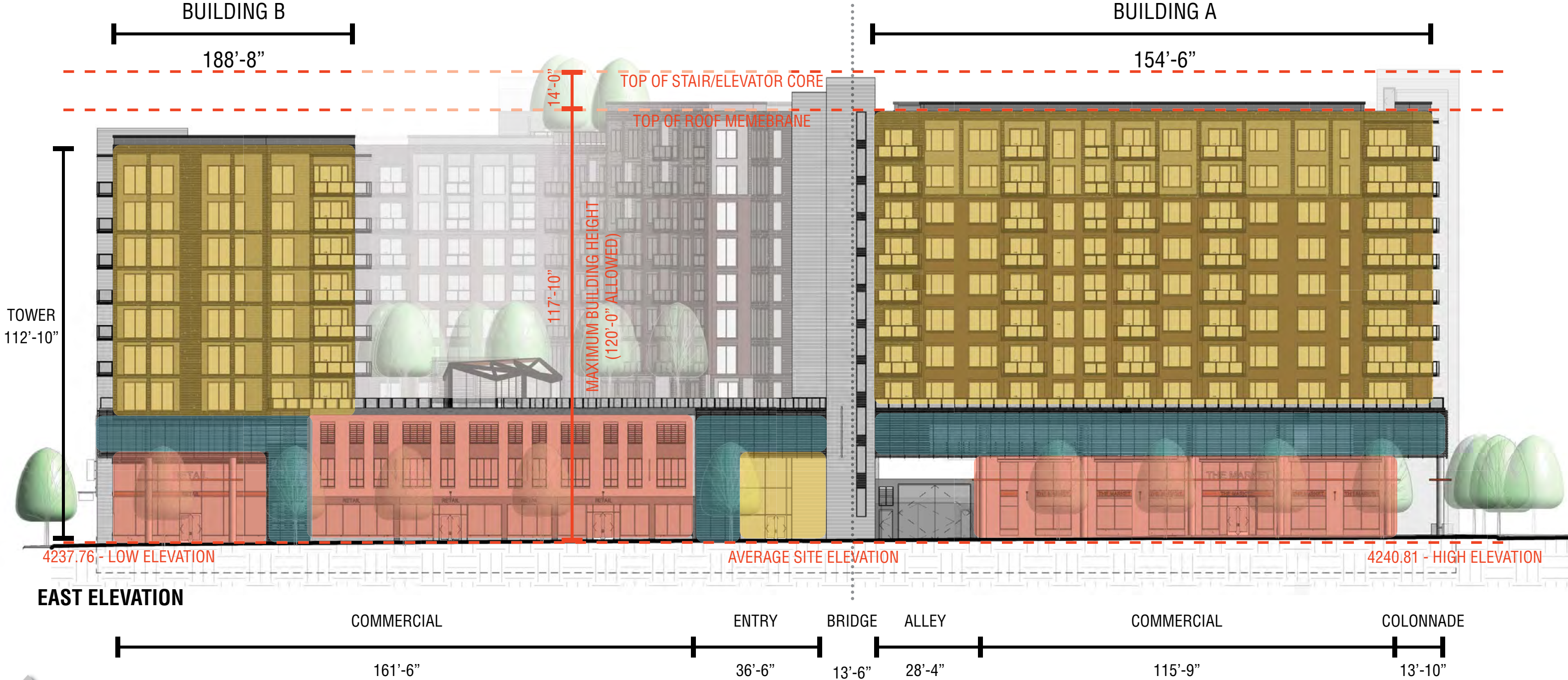
FUTURE PUBLIC STREET





# ELEVATION COMPLIANCE DIAGRAM | CODE & ZONING

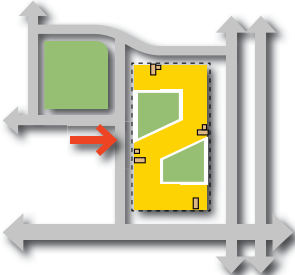
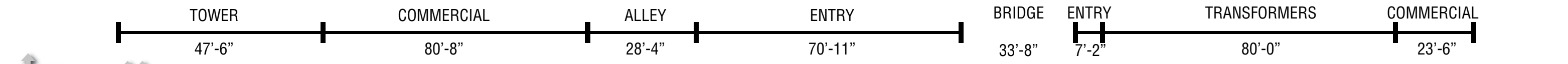
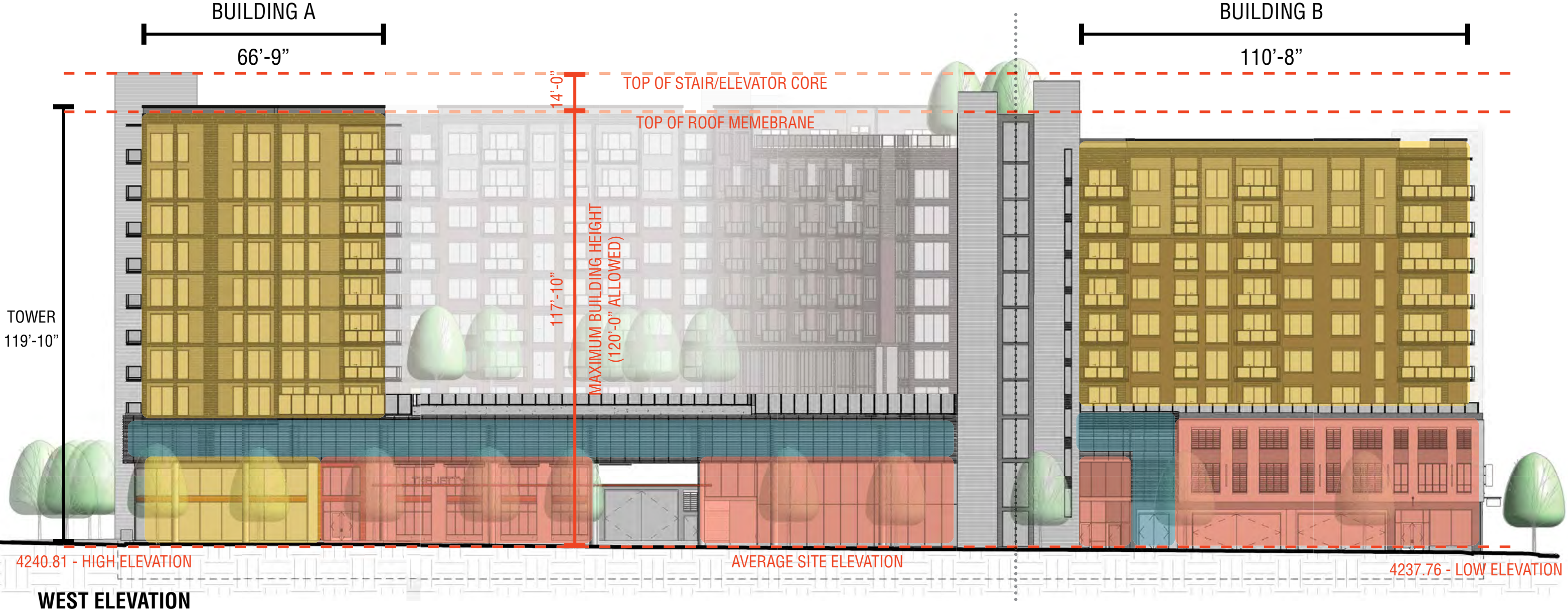
**21A.37.050.F** MAXIMUM LENGTH OF STREET FACING FACADE  
**21A.30.030.D** MAXIMUM BUILDING HEIGHT





# ELEVATION COMPLIANCE DIAGRAM | CODE & ZONING

**21A.37.050.F** MAXIMUM LENGTH OF STREET FACING FACADE  
**21A.30.030.D** MAXIMUM BUILDING HEIGHT





# ELEVATION COMPLIANCE DIAGRAM | CODE & ZONING

## 21A.37.050.F MAXIMUM LENGTH OF STREET FACING FACADE

BUILDING A

192'-8"

TOWER  
119'-10"

NORTH ELEVATION

COMMERCIAL

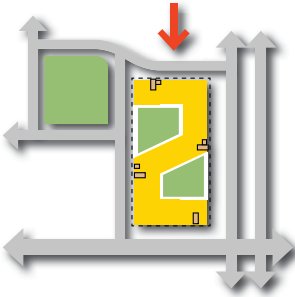
BRIDGE

TOWER

137'-9"

23'-9"

28'-0"



Standard (Code Section)	District			
	D-1	D-2	D-3	D-4
Ground floor use (%) (21A.37.050A1)		75		75 <sup>3</sup>
Ground floor use + visual interest (%) (21A.37.050A2)		60/25		
Building materials: ground floor (%) (21A.37.050B1)		80	70 <sup>2</sup>	
Building materials: upper floors (%) (21A.37.050B2)		50	70 <sup>2</sup>	
Glass: ground floor (%) (21A.37.050C1)	40/60 <sup>1</sup>	40	40	40
Glass: upper floors (%) (21A.37.050C2)		25		
Building entrances (feet) (21A.37.050D)		50		
Blank wall: maximum length (feet) (21A.37.050E)		15		
Street facing facade: maximum length (feet) (21A.37.050F)		200		
Upper floor step back (feet) (21A.37.050G)				
Lighting: exterior (21A.37.050H)		X		
Lighting: parking lot (21A.37.050I)		X		
Screening of mechanical equipment (21A.37.050J)		X		
Screening of service areas (21A.37.050K)		X		
Ground floor residential entrances (21A.37.050L)				
Parking garages or structures (21A.37.050M)		X		

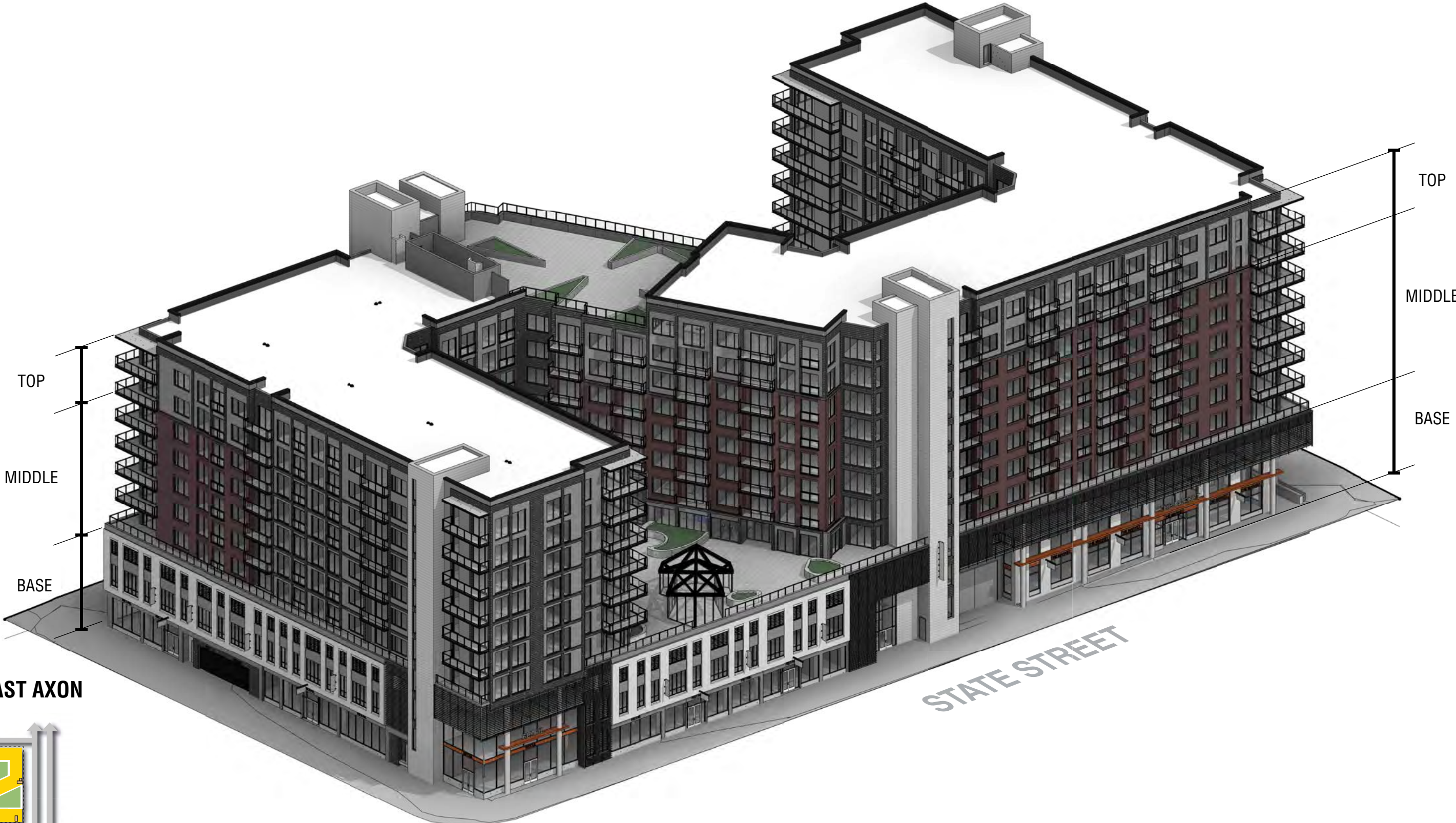
**Notes:**

1. Minimum requirement is 60 percent when project is within the Main Street retail core.
2. In the D-3 Zoning District this percentage applies to all sides of the building, not just the front or street facing facade.
3. This percentage applies only as a requirement as noted in subsection 21A.30.045C7b of this title for projects that are seeking conditional height.

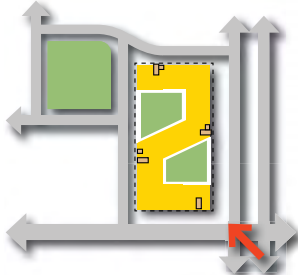


# COMPLIANCE DIAGRAM | CODE & ZONING

BASE, MIDDLE, & TOP



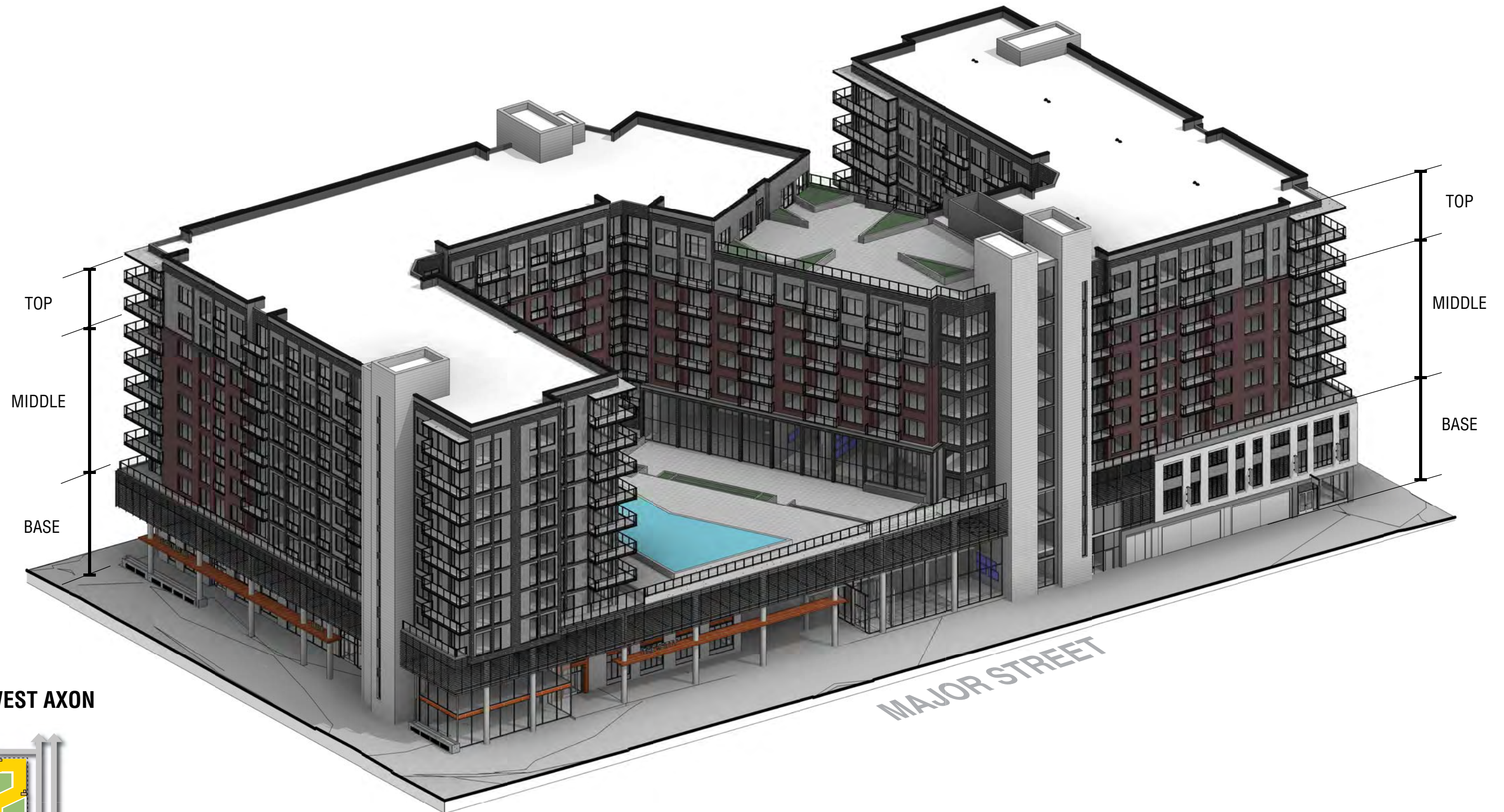
**SOUTHEAST AXON**



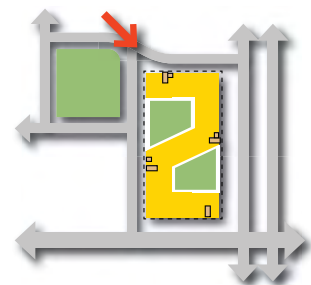


# COMPLIANCE DIAGRAM | CODE & ZONING

BASE, MIDDLE, & TOP



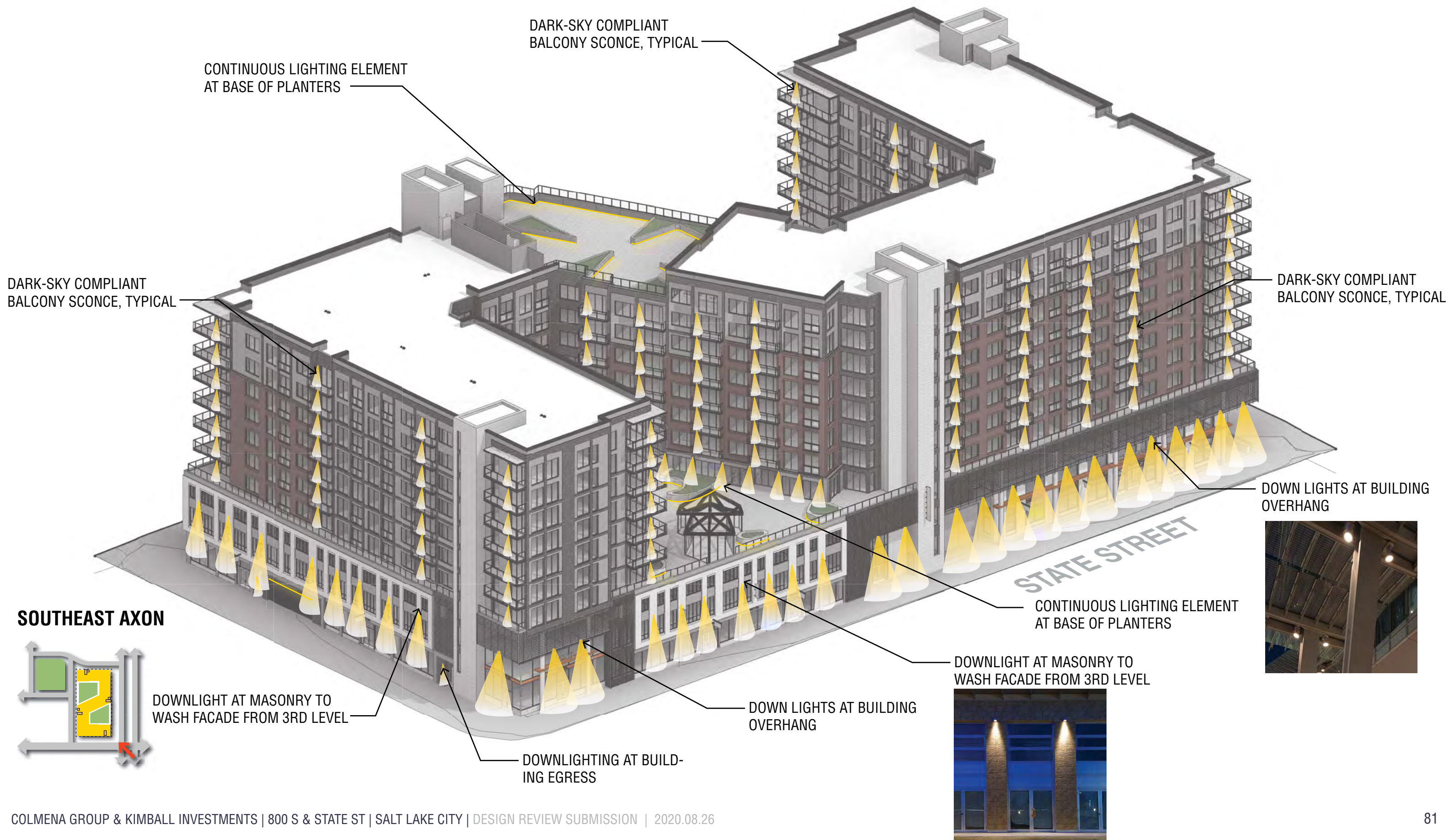
**NORTHWEST AXON**





# LIGHTING INTENT | CODE & ZONING

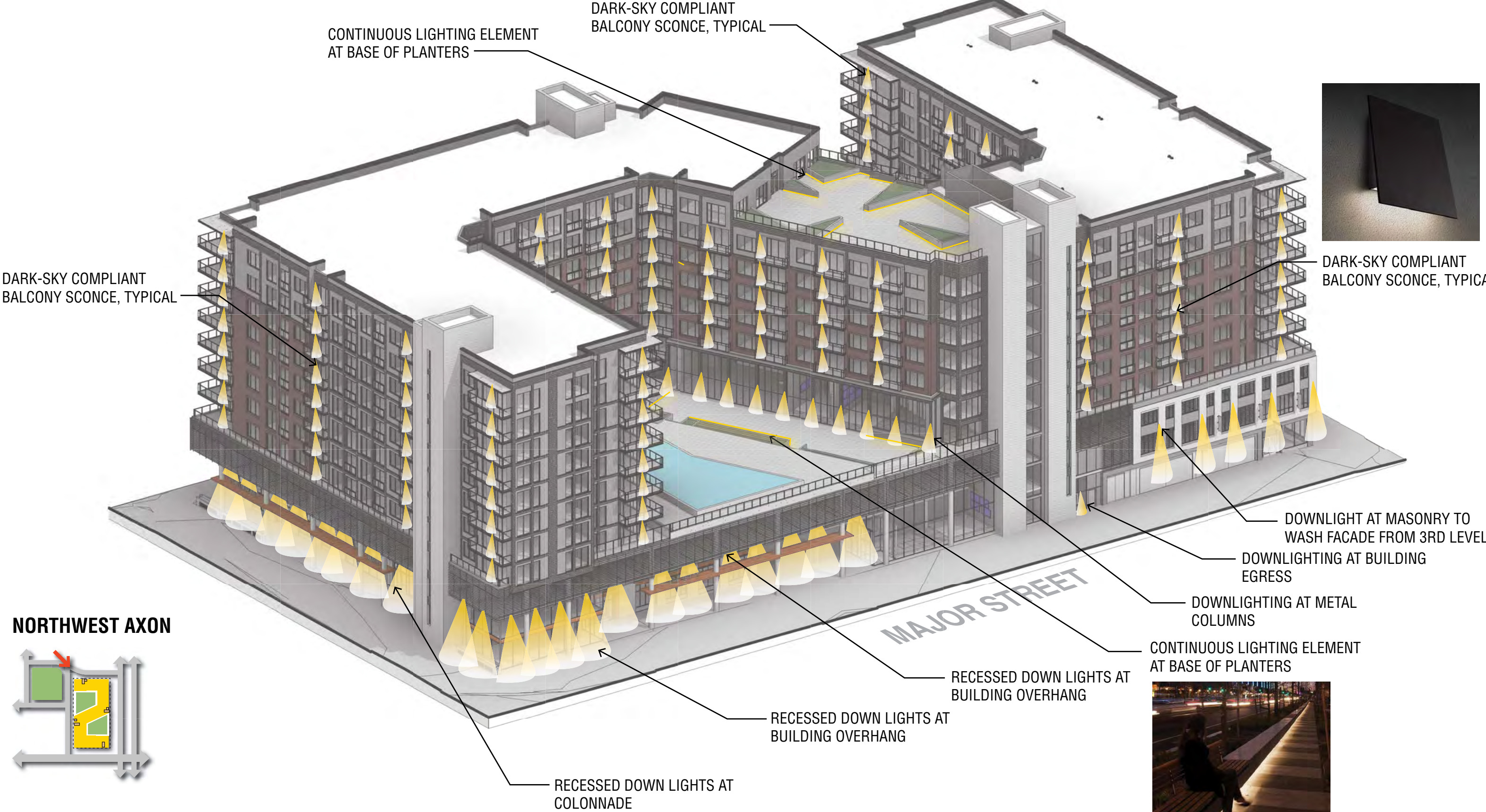
21A.37.050.H EXTERIOR LIGHTING





# LIGHTING INTENT | CODE & ZONING

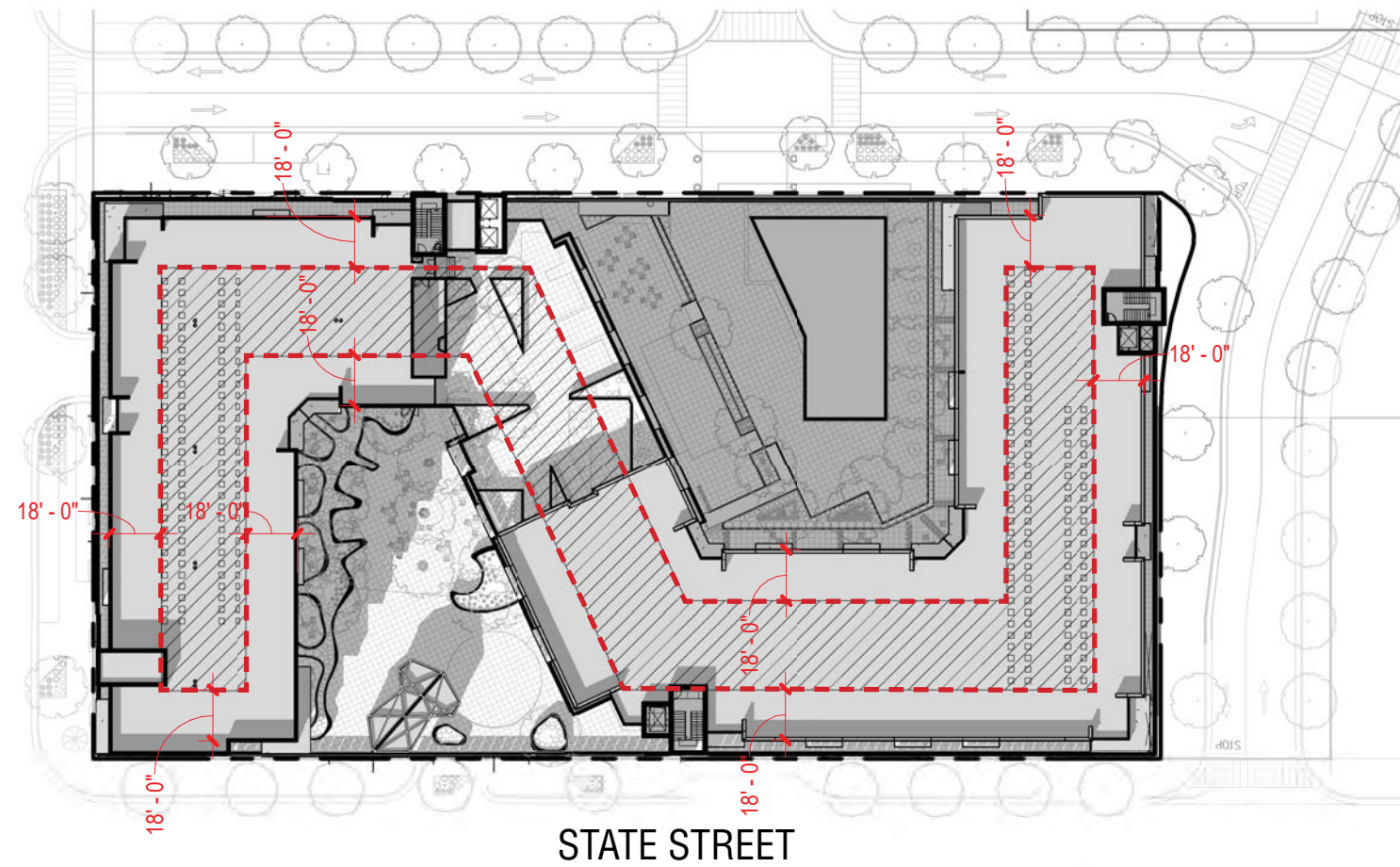
21A.37.050.H EXTERIOR LIGHTING





# ROOF EQUIPMENT VISIBILITY | CODE & ZONING

21A.37.050.J SCREENING OF MECHANICAL EQUIPMENT



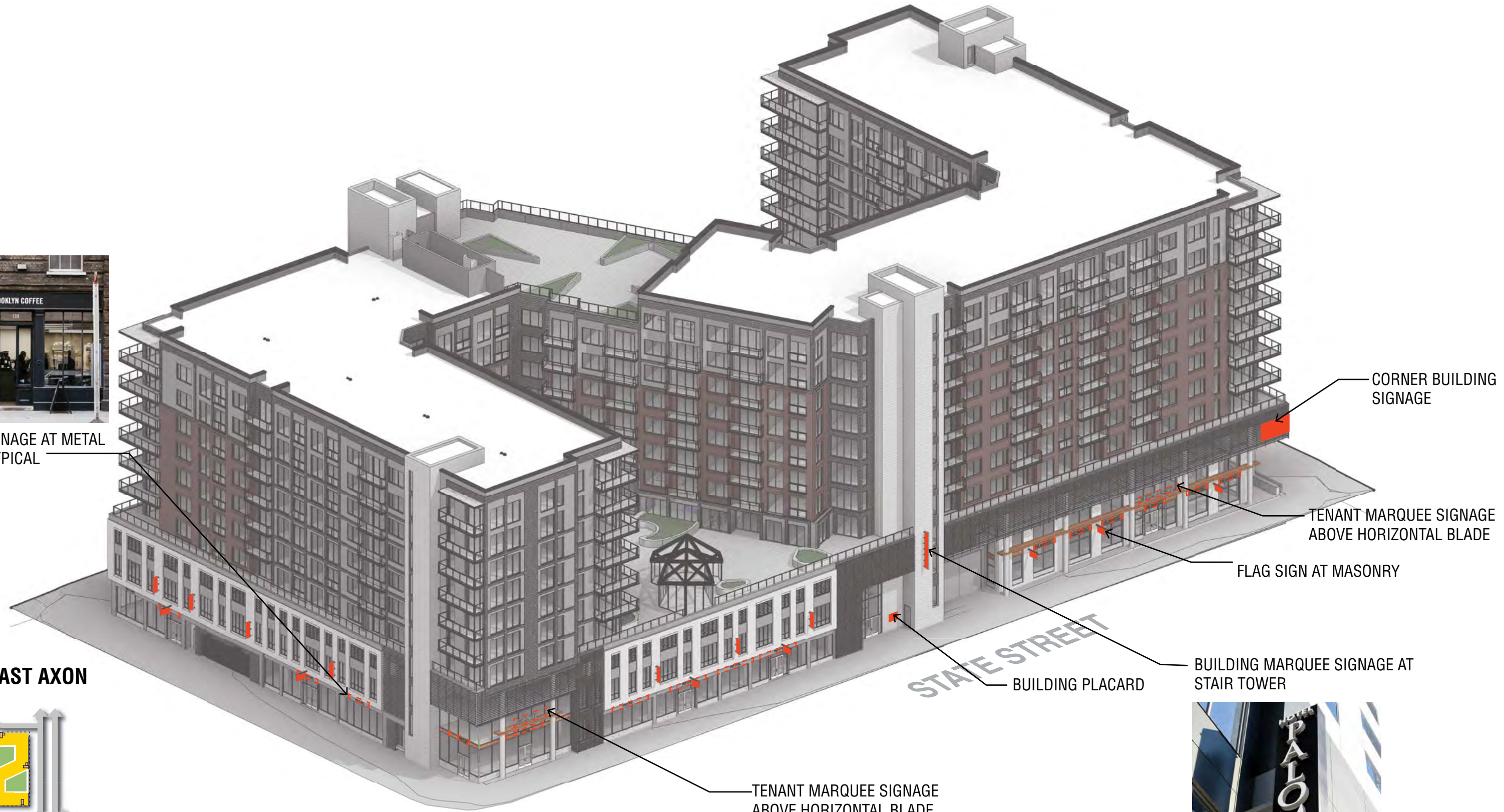


# SIGNAGE OPPORTUNITIES | CODE & ZONING

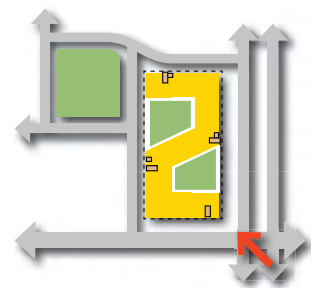
21A.46.110.B SIGN REGULATIONS FOR THE D-2 DISTRICT



TENANT SIGNAGE AT METAL RETURN, TYPICAL



## SOUTHEAST AXON





# SIGNAGE OPPORTUNITIES | CODE & ZONING

21A.46.110.B SIGN REGULATIONS FOR THE D-2 DISTRICT





## SLC 21A.37 Design Standards Compatibility/Compliance

<p><b><u>A. Ground Floor Use and Visual Interest</u></b></p> <p>This standard's purpose is to increase the amount of active uses and/or visual interest on the ground floor of a building. There are two (2) options for achieving this, one dealing solely with the amount of ground floor use, and the other combining a lesser amount of ground floor use with increased visual interest in the building facade's design.</p> <ol style="list-style-type: none"> <li>1. <b>Ground Floor Use Only:</b> This option requires that on the ground floor of a new principal building, a permitted or conditional use other than parking shall occupy a minimum portion of the length of any street facing building facade according to section <a href="#">21A.37.060</a>, table 21A.37.060 of this chapter. All portions of such ground floor spaces shall extend a <b>minimum of twenty-five feet (25') into the building.</b> Parking may be located behind these spaces.             <ol style="list-style-type: none"> <li>a. For single-family attached uses, the required use depth may be reduced to ten feet (10').</li> <li>b. For single-family or two-family uses, garages occupying up to fifty percent (50%) of the width of the ground floor building facade are exempt from this requirement.</li> <li>c. For all other uses, vehicle entry and exit ways necessary for access to parking are exempt from this requirement. Such <b>accessways shall not exceed thirty feet (30') in width.</b> Individual dwelling unit garages do not qualify for this exemption.</li> </ol> </li> <li>2. <b>Ground Floor Use And Visual Interest:</b> This option allows for some flexibility in the amount of required ground floor use, but in return requires additional design requirements for the purpose of creating increased visual interest and pedestrian activity where the lower levels of buildings face streets or sidewalks. An applicant utilizing this option <u>must proceed through the design review process</u> for review of the project for determination of the project's compliance with those standards, and in addition, whether it contributes to increased visual interest through a combination of increased building material variety, architectural features, facade changes, art, and colors; and, increased pedestrian activity through permeability between the building and the adjacent public realm using niches, bays, gateways, porches, colonnades, stairs or other similar features to facilitate pedestrian interaction with the building.</li> </ol>	<ol style="list-style-type: none"> <li>1. Ground floor use: The proposed development takes advantage of most of the ground level interaction to the street by creating an interesting storefront interaction that engages the sidewalk with multiple uses including retail storefronts, colonnades with active seating opportunities, and entry porches to the live- work units.             <ul style="list-style-type: none"> <li>▪ All ground floor uses have a minimum depth of 25' -0"</li> <li>▪ Accessways (to service bay and garage entry) are a max. width of 30'-0"</li> <li>▪ The retail parking is connected to the public sidewalks at several portals to the building to facilitate the flow to the retail/commercial spaces as well as the live-work units.</li> <li>▪ Signage, way finding, and street entourage (seating areas, planter boxes, etc.), are planned to energize the neighborhood experience.</li> </ul> </li> <li>2. Visual Interest: The base of the building creates a plinth from the ground level up to the bottom of the 4<sup>th</sup> level amenities spaces. Forming an interesting and energized pedestrian scale, the plinth reinforces the engagement of the building with the street. We are proposing rich materials inclusive of brick masonry, exposed form-lined concrete panels, and transparent storefront windows with many façade changes along the building front to increase the pedestrian interaction with the building.</li> </ol>
<p><b><u>B. Building Materials:</u></b></p> <ol style="list-style-type: none"> <li>1. Ground Floor Building Materials: Other than windows and doors, a minimum amount of the ground floor facade's wall area of any street facing facade shall be clad in durable materials according to section <a href="#">21A.37.060</a>, table 21A.37.060 of this chapter. Durable materials include stone, brick, masonry, textured or patterned concrete, and fiber cement board. Other materials may be used for the remainder of the ground floor facade adjacent to a street. Other materials proposed to satisfy the durable requirement may be approved at the discretion of the Planning Director if it is found that the proposed material is durable and is appropriate for the ground floor of a structure.</li> <li>2. Upper Floor Building Materials: Floors above the ground floor level shall include durable materials on a minimum amount of any street facing building facade of those additional floors according to section <a href="#">21A.37.060</a>, table 21A.37.060 of this chapter. Windows and doors are not included in that minimum amount. Durable materials include stone, brick, masonry, textured or patterned concrete, and fiber cement board. Other materials may be approved at the discretion of the Planning Director if it is found that the proposed material is durable and is appropriate for the upper floor of a structure.</li> </ol>	<ol style="list-style-type: none"> <li>1. Ground Floor Materials: consist of predominantly brick masonry, textured concrete, and insulated metal panels. To provide more of an open, transparent, and welcoming feel, the retail and the residential common areas (lobbies &amp; leasing) will be tall storefront (aluminum/glass).</li> <li>2. Upper floors will consist of metal panels as well as fiber cement panels and to help ground the building and the use of brick masonry for the residential units will continue above the podium level as major elements and as accents.</li> </ol>
<p><b><u>C. Glass:</u></b></p> <ol style="list-style-type: none"> <li>A. Ground Floor Glass: The ground floor building elevation of all new buildings facing a street, and all new ground floor additions facing a street, shall have a minimum amount of glass, or within a specified percentage range, between three feet (3') and eight feet (8') above grade according to section <a href="#">21A.37.060</a>, table 21A.37.060 of this chapter. All ground floor glass shall allow unhampered and unobstructed visibility into the building for a depth of at least five feet (5'), excluding any glass etching and window signs when installed and permitted in</li> </ol>	<ol style="list-style-type: none"> <li>A. The porosity of the building at the ground levels (1<sup>st</sup> and 2<sup>nd</sup> levels) is intended to increase the visibility of the inside with the goal of bringing the inside out and vice versa. The percentage amount of glazing of the ground floor will be a minimum of 40% as required.</li> </ol>



# DESIGN STANDARDS COMPLIANCE | CODE & ZONING

<p>accordance with chapter 21A.46, "Signs", of this title. The Planning Director may approve a modification to ground floor glass requirements if the Planning Director finds:</p> <ol style="list-style-type: none"> <li>a. The requirement would negatively affect the historic character of an existing building.</li> <li>b. The requirement would negatively affect the structural stability of an existing building; or</li> <li>c. The ground level of the building is occupied by residential uses that face the street; in which case the specified minimum glass requirement may be reduced by fifteen percent (15%).</li> </ol> <p>B. Upper Floor Glass: Above the first floor of any multi-story building, the surface area of the facade of each floor facing a street must contain a minimum amount of glass according to section <a href="#">21A.37.060</a>, table 21A.37.060 of this chapter.</p>	<p>B. The upper level window wall has a more residential feel, with the articulation of rich punched windows to increase the interest of the building in the upper levels. The percentage amount of glazing on the upper floors will be a minimum of 25%.</p>
<p><b><u>D. Building Entrances:</u></b></p> <p>At least one operable building entrance on the ground floor is required for every street facing facade. Additional operable building entrances shall be required, at a minimum, at each specified length of street facing building facade according to section <a href="#">21A.37.060</a>, table 21A.37.060 of this chapter. The center of each additional entrance shall be located within six feet (6') either direction of the specified location. Each ground floor nonresidential leasable space facing a street shall have an operable entrance facing that street and a walkway to the nearest sidewalk. Corner entrances, when facing a street and located at approximately a forty-five degree (45°) angle to the two (2) adjacent building facades (chamfered corner), may count as an entrance for both of the adjacent facades.</p>	<p>Building entrances are located on all sides of the building (facing all surrounding streets). All entry locations are located on the edge of the property line just inside of the public right of way, which allows direct interaction with the street level. Building entries are dispersed along façade based on the intent of the zoning required spacing but based on programmatic needs in select locations (D-2 zone requires a max distance of 50').</p> <p>A service entry is provided along the west side of the building aligned with the street in front of the future park. The service yard inside the building will act as a one-way alley that disembarks on State Street with a right-out only connection for service vehicles.</p>
<p><b><u>E. Maximum Length of Blank Wall</u></b></p> <p>The maximum length of any blank wall uninterrupted by windows, doors, art or architectural detailing at the ground floor level along any street facing facade shall be as specified according to section <a href="#">21A.37.060</a>, table 21A.37.060 of this chapter. Changes in plane, texture, materials, scale of materials, patterns, art, or other architectural detailing are acceptable methods to create variety and scale. This shall include architectural features such as bay windows, recessed or projected entrances or windows, balconies, cornices, columns, or other similar architectural features. The architectural feature shall be either <u>recessed a minimum of twelve inches (12")</u> or <u>projected a minimum of twelve inches (12")</u>.</p>	<p><b>D-2 Zone, max 15'</b> Street level wall planes allow for a variety of depths (patios/entry/driveway). The use of masonry pilasters also provides play of the façade depth horizontally on the building.</p> <p>For the articulation of entry portals and service yards gates, we are proposing art gates inspired by local artists in the community.</p>
<p><b><u>F. Maximum Length Of Street Facing Facades</u></b></p> <p>No street facing building wall may be longer than specified along a street line according to section <a href="#">21A.37.060</a>, table 21A.37.060 of this chapter. A minimum of twenty feet (20') is required between separate buildings when multiple buildings are placed on a single parcel according to subsection <a href="#">21A.36.010B</a>, "One Principal Building Per Lot", of this title. The space between buildings shall include a pedestrian walkway at least five feet (5') wide. <b>D-2 Zone: 200' max</b></p>	<p><b>D-2 Zone max 200'</b> The max length of a continuous wall (without a break in massing) is less than 200' (set at 196').</p> <p>The building façade is broken in elements encouraged by this design standard to help us break the building (horizontal and vertical) mass as well as to allow us to articulate a façade that has the impression of being several buildings. Live/work units to the south of the building maintain a distinct aesthetic from the more typical retail functions of the north section. In addition, we are introducing a through-street that will service as a service alley that will be secured with a recessed decorative metal gate.</p>



<p><b><u>G. Upper Floor Step Back</u></b></p> <ol style="list-style-type: none"> <li>1. For street facing facades the first full floor, and all additional floors, above thirty feet (30') in height from average finished grade shall be stepped back a minimum horizontal distance from the front line of building, according to section <a href="#">21A.37.060</a>, table 21A.37.060 of this chapter. An alternative to this street facing façade step back requirement may be utilized for buildings limited to forty five feet (45') or less in height by the zoning ordinance: those buildings may provide a four foot (4') minimum depth canopy, roof structure, or balcony that extends from the face of the building toward the street at a height of between twelve feet (12') and fifteen feet (15') above the adjacent sidewalk. Such extension(s) shall extend horizontally parallel to the street for a minimum of fifty percent (50%) of the face of the building and may encroach into a setback as permitted per section <a href="#">21A.36.020</a>, table 21A.36.020B, "Obstructions In Required Yards", of this title.</li> <li>2. For facades facing single- or two-family residential districts, a public trail or public open space the first full floor, and all additional floors, above thirty feet (30') in height from average finished grade shall be stepped back a minimum horizontal distance from the corresponding required yard setback (building line) according to section <a href="#">21A.37.060</a>, table 21A.37.060 of this chapter.</li> </ol>	<p>This design standard is not applicable at the project location. However, note that the building façade is articulating Upper Floor Step Backs to create interest to the building bulk and mass.</p>
<p><b><u>H. Exterior Lighting</u></b></p> <p>All exterior lighting shall be shielded and directed down to prevent light trespass onto adjacent properties. Exterior lighting shall not strobe, flash or flicker.</p>	<p>Exterior lighting will comply with D2 general regulations accentuating the safety of the pedestrian activity around the building and will be careful to maintain the night sky in the context of upper deck amenities. Photometric will be provided during the review process.</p>
<p><b><u>I. Parking Lot Lighting</u></b></p> <p>If a parking lot/structure is adjacent to a residential zoning district or land use, any poles for the parking lot/structure security lighting are limited to sixteen feet (16') in height and the globe must be shielded and the lighting directed down to minimize light encroachment onto adjacent residential properties or into upper level residential units in multi-story buildings. Lightproof fencing is required adjacent to residential properties.</p>	<p>All parking garages will be screened from street view. Clear signage and wayfinding will be provided to easily identify the parking garage entries.</p>
<p><b><u>J. Screening Of Mechanical Equipment</u></b></p> <p>All mechanical equipment for a building shall be screened from public view and sited to minimize their visibility and impact. Examples of siting include on the roof, enclosed or otherwise integrated into the architectural design of the building, or in a rear or side yard area subject to yard location restrictions found in section <a href="#">21A.36.020</a>, table 21A.36.020B, "Obstructions In Required Yards", of this title.</p>	<p>All mechanical equipment is located on the rooftop and are screened from view. Utilities on street level are located behind gates and screened from view.</p>
<p><b><u>K. Screening Of Service Areas</u></b></p> <p><b>Service areas, loading docks, refuse containers and similar areas shall be fully screened from public view.</b> All screening enclosures viewable from the street shall be either incorporated into the building architecture or shall incorporate building materials and detailing compatible with the building being served. All screening devices shall be a minimum of one foot (1') higher than the object being screened, and in the case of fences and/or masonry walls the height shall not exceed eight feet (8'). Dumpsters must be located a minimum of twenty-five feet (25') from any building on an adjacent lot that contains a residential dwelling or be located inside of an enclosed building or structure.</p>	<p>A dedicated and centralized service yard is in the middle of the building acting as an internal alley way. This enclosed service yard will serve both commercial and residential uses (trash, deliveries, move-in, maintenance). The enclosed service yard independently accessed through artistically inspired gates, is planned as one-way traffic, and will be protected, screened, and controlled.</p>
<p><b><u>L. Ground Floor Residential Entrances For Single-Family Dwellings</u></b></p> <p>For the zoning districts listed in section <a href="#">21A.37.060</a>, table 21A.37.060 of this chapter all attached single-family dwellings, townhomes, row houses, and other similar single-family housing types located on the ground floor shall have a <b>primary entrance facing the street</b> for each unit adjacent to a street. Units may have a primary entrance located on a courtyard, mid-block walkway, or other similar area if the street facing facades also have a primary entrance.</p>	<p>(not applicable)</p>



<p><b><u>M. Parking Garages or Structures</u></b></p> <p>The following standards shall apply to parking garages or structures whether stand alone or incorporated into a building:</p> <ol style="list-style-type: none"> <li>1. Parking structures shall have an <u>external skin designed</u> to improve visual character when adjacent to a public street or other public space. Examples include heavy gauge metal screen, precast concrete panels; live green or landscaped walls, laminated or safety glass, decorative photovoltaic panels or match the building materials and character of the principal use. The planning director may approve other decorative materials not listed if the materials are in keeping with the decorative nature of the parking structure.</li> <li>2. The architectural design of the facades should express the internal function of the structure. Facade elements shall align to parking levels and there shall be no sloped surfaces visible from a public street, public trail or public open space.</li> <li>3. <u>Internal circulation</u> must be designed such that parking surfaces are level (or without any slopes) along all primary facades. All ramping between levels need to be placed along the secondary facade or to the center of the structure. Parking structures shall be designed to conceal the view of all parked cars and drive ramps from public spaces.</li> <li>4. Elevator and stairs shall be <u>highlighted architecturally</u> so visitors, internally and externally, can easily access these entry points.</li> <li>5. <u>Signage and wayfinding</u> shall be integrated with the architecture of the parking structure and be architecturally compatible with the design. Public parking structures entrances shall be clearly signed from public streets.</li> <li>6. Interior garage lighting shall not produce glaring sources toward adjacent properties while providing safe and adequate lighting levels. The use of sensor dimmable LEDs and white stained ceilings are a good strategy to control light levels on site while improving energy efficiency.</li> <li>7. Where a <u>driveway crosses a public sidewalk</u>, the driveway shall be a different color, texture, or paving material than the sidewalk to warn drivers of the possibility of pedestrians in the area.</li> <li>8. The street level facing facades of all parking structures <u>shall be wrapped along</u> all street frontages with habitable space that is occupied by a use that is allowed in the zone as a permitted or conditional use.</li> <li>9. Parking structures shall be designed to minimize vehicle noise and odors on the public realm. <u>Venting and fan locations shall not be located next to public spaces</u> and shall be located as far as possible from adjacent residential land uses.</li> </ol>	<ol style="list-style-type: none"> <li>1. The garage level 3, the only garage level that comes to the edge of the building, is proposed to be screened as an integral part of the overall architectural concept. It creates a horizontal base/plinth for the residential massing located above (level 4-11).</li> <li>2. The screen conceals all vehicular parking spaces, and parking level lighting, as well allows ventilation of the garage and no sloped surfaces shall be visible from the street.</li> <li>3. All ramping occurs internally. All garage circulation is internalized</li> <li>4. All elevator and stair cores occur along the perimeter of the building and become an identifiable element of the building structure connecting all levels of the building.</li> <li>5. Potential locations of building signage are positioned throughout the perimeter of the building (see signage diagram).</li> <li>6. Interior garage lighting will be controlled by its intensity as well as by the screening along the perimeter, to prevent glare to the outside.</li> <li>7. Driveways that intersect public sidewalks will be clearly marked with distinct colors and patterns and separated by bollards (with integral down-lighting). Driveways will also be raised to the same height as the sidewalks</li> <li>8. All street level facades are wrapped by habitable uses (live/work) and public common areas (lobby, lounges, etc.)</li> <li>9. All garage levels are naturally and mechanically ventilated as required. The location of air intake for ventilation is on the ground level and exhaust is on the third level perimeter far away from public spaces and placed to prevent short circuiting of the system.</li> </ol>
<p><b><u>N. Residential Character In RB District</u></b></p> <ol style="list-style-type: none"> <li>1. All roofs shall be pitched and of a hip or gable design except additions or expansions to existing buildings may be of the same roof design as the original building.</li> <li>2. The remodeling of residential buildings for retail or office use shall be allowed only if the residential character of the exterior is maintained.</li> <li>3. The front building elevation shall contain no more than fifty percent (50%) glass.</li> <li>4. Signs shall conform with special sign regulations of chapter 21A.46, "Signs", of this title.</li> <li>5. Building orientation shall be to the front or corner side yard; and</li> <li>6. Building additions shall consist of materials, color and exterior building design consistent with the existing structure, unless the entire structure is resurfaced.</li> </ol>	<p style="text-align: center;">RB (Residential Business) (not applicable)</p>
<p><b><u>O. Primary Entrance Design In SNB District</u></b></p> <p>Primary entrance design shall consist of at least two (2) of the following design elements at the primary entrance, so that the primary entrance is architecturally prominent and clearly visible from the abutting street.</p> <ol style="list-style-type: none"> <li>1. Architectural details such as arches, friezes, tile work, canopies, or awnings.</li> <li>2. Integral planters or wing walls that incorporate landscape or seating.</li> <li>3. Enhanced exterior light fixtures such as wall sconces, light coves with concealed light sources, or decorative pedestal lights.</li> <li>4. A repeating pattern of pilasters projecting from the facade wall by a minimum of eight inches (8") or architectural or decorative columns.</li> <li>5. Recessed entrances that include a minimum step back of two feet (2') from the primary facade and that include glass on the sidewalls. (Ord. 14-19, 2019; Ord. 12-17, 2017)</li> </ol>	<p style="text-align: center;">SNB (Small Neighborhood Business) (not applicable)</p>



# THE SITE

## **Overview:**

The phase 1 Sears Block project will consist of site improvements, public road improvements and utility main improvements located at the northwest corner of State Street and 800 South. The site will consist of 1.74 acres which will be almost entirely building. There will also be approximately 1 acre of new public roads, 60 foot road right of way, along the North and West sides of the proposed site. The new public roads will also include water main extensions and some sewer main and storm drain extensions. There will also be a storm drain main extension from the intersection of 700 South and Main Street to the site.

## **Site Improvements:**

The proposed building footprint will cover the entire site except for some concrete sidewalk work adjacent to the building, where the building is set back slightly from the property line.

## **Public Road Improvements:**

There will be two 66 foot road right of way roads as part of this project. The road running east-west, from State Street, along the north portion of the site will be approximately 280 feet in length. The proposed cross section of the streets is 24 feet of asphalt, 2.5 feet of curb and gutter on each side of the road and the remaining 31 feet will be mostly concrete sidewalk with some sidewalk areas being a more “decorative or distinct” surface. As part of the road improvements, street trees, street signs, pavement markings, ADA ramps and street lights will all be required. The road running North-South, from 800 South Street, along the west portion of the site will be approximately 450 feet in length.

State Street and 800 South road improvements. These two roads are existing and border the site on the east and south sides. On State Street there will be existing drive approaches that will need to

be removed and new curb & gutter and sidewalk installed. All this work will need to be done per UDOT standards and an encroachment permit will be required from UDOT. There will be one new drive approach from State Street and the new public road running along the north side of the site will connect to State Street. It is anticipated that most of the existing sidewalk between existing curb and the site will need to be replaced due to damage during construction of the building. All existing street signs, street lights, utility structures, etc... along State Street will need to be protected/replaced and or relocated as part of this project. 800 South borders the site on the south. There will be one drive approach installed and the new public road running along the west side of the site will connect to 800 South. All work in 800 South will be per Salt Lake City/APWA Standards and Specifications. A permit will be required from SLC Engineering for all work in the road right of way. It is



anticipated that most of the existing sidewalk between existing curb and the site will need to be replaced due to damage during construction of the building. All existing street signs, street lights, utility structures, etc... along 800 South will need to be protected/replaced and or relocated as part of this project.

**Utility Improvements:**

Utility stubs to the building will consist of two, 8” fire protection lines from an upsized water main in state street. A gate valve on the main, separating the two 8” laterals will be required. It is anticipated that a 6” culinary water lateral, with meter and meter vault will be required. It is anticipated that four 8” sewer laterals will be required. Two of these laterals will require a new sewer manhole installation on an existing main. One will be from an existing sewer manhole and one will be from a sewer main extension that is part of this project and discussed below.

All roof storm water runoff will be collected and discharged to a storm drain lateral provided on the West side of the site that will direct storm water to a regional detention area. The regional storm water detention area is proposed to be an underground storm tech system located just west of the site and the storm water treatment device will also be located there.

**Utility Main Improvements:**

Water, sewer and storm drain main improvements will be required with this project. Water Mains in State Street and 800 South are existing 6” mains. Along the entire frontage of the property on both roads, it will be required to upsize these mains to 12”. All road cuts and repairs, gate valves, thrust block and traffic control needed to complete this work should be included. For the new public roads, an 8” water main will be provided, including fire hydrants.

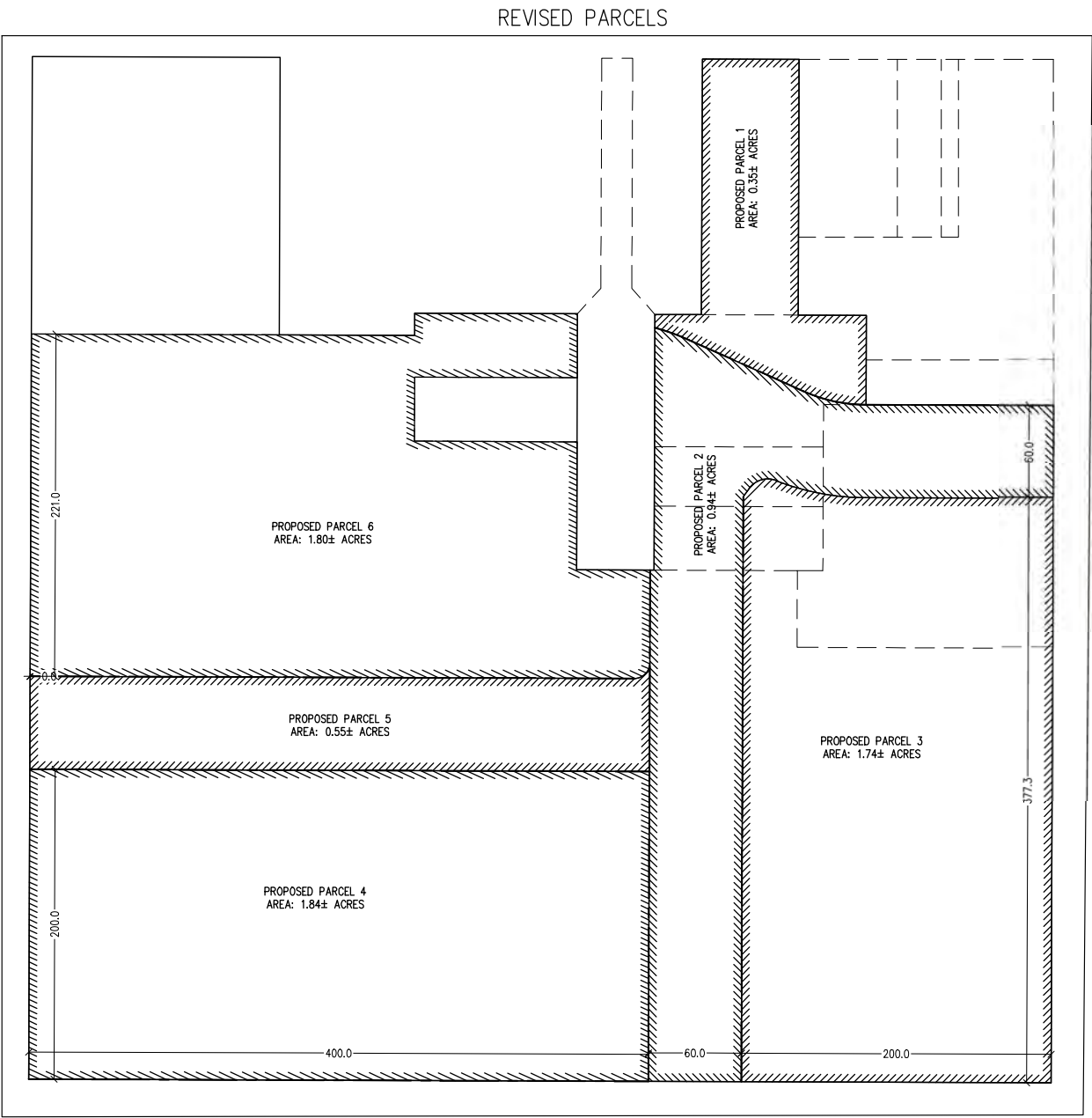
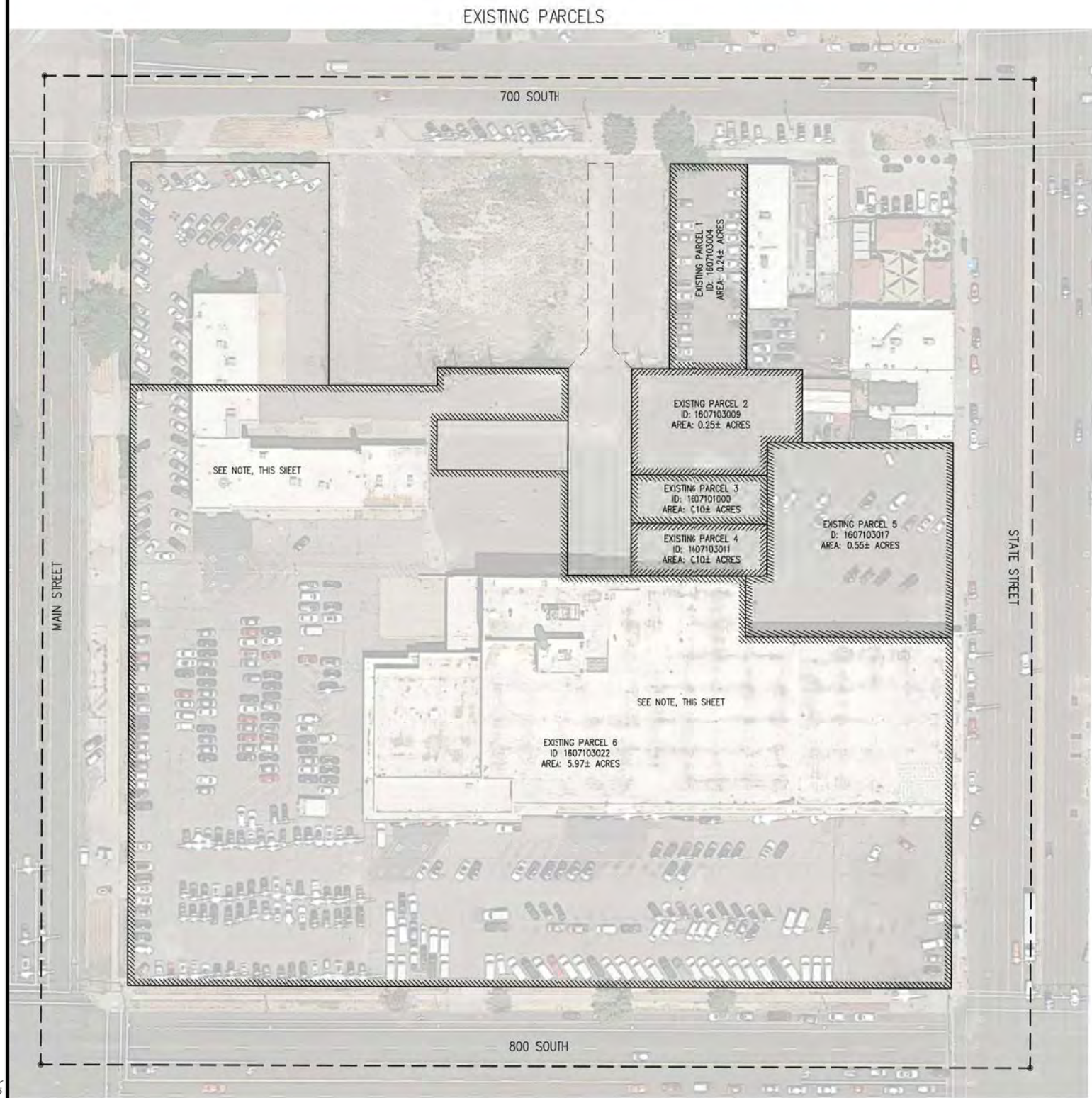
Sewer Main improvements will consist of a new 8” sewer main

running from 800 South, north the majority of the length of the new road. This sewer main will service the proposed building as well as future development to the west of this site. This should include all piping, manholes, laterals and backfill material required to complete this work.

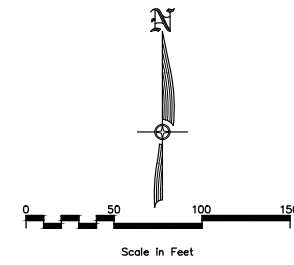
Storm Drain Main improvements will consist of running an 18” RCP pipe from the intersection of 800 South and Main Street, running North in Main Street for approximately 285 feet and then East for approximately 305 feet. It will be required to provide a new storm drain manhole at an existing 30” storm drain main in 800 South and replacing one storm drain structure at that same intersection. Additional manholes will be provided to accommodate future improvements. All road cuts and repairs, storm drain structures, piping, backfill material and traffic control needed to complete this work should be included. At the connection points of the new

roads to State Street and 800 South, both have some storm drain main improvements required to collect storm water runoff from the road.





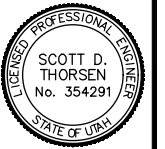
NOTE:  
AS PART OF PARCEL ADJUSTMENT, DEMOLITION OF THE TWO EXISTING SEARS BUILDINGS WILL BE REQUESTED.



NO.	REVISIONS	BY	DATE

**CIVIL ENGINEERING SURVEYING**  
**GIR**  
 3032 SOUTH 1030 WEST, SUITE 202  
 SLC, Utah 84119 - 801-949-6296  
 DESIGNER: SDT PROJECT ENGINEER: SDT

SD SET - SEARS BLOCK - PHASE 1  
 750 SOUTH STATE STREET, SLC, UTAH  
 EXHIBIT PARCEL ADJUSTMENT



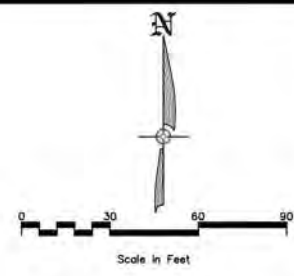
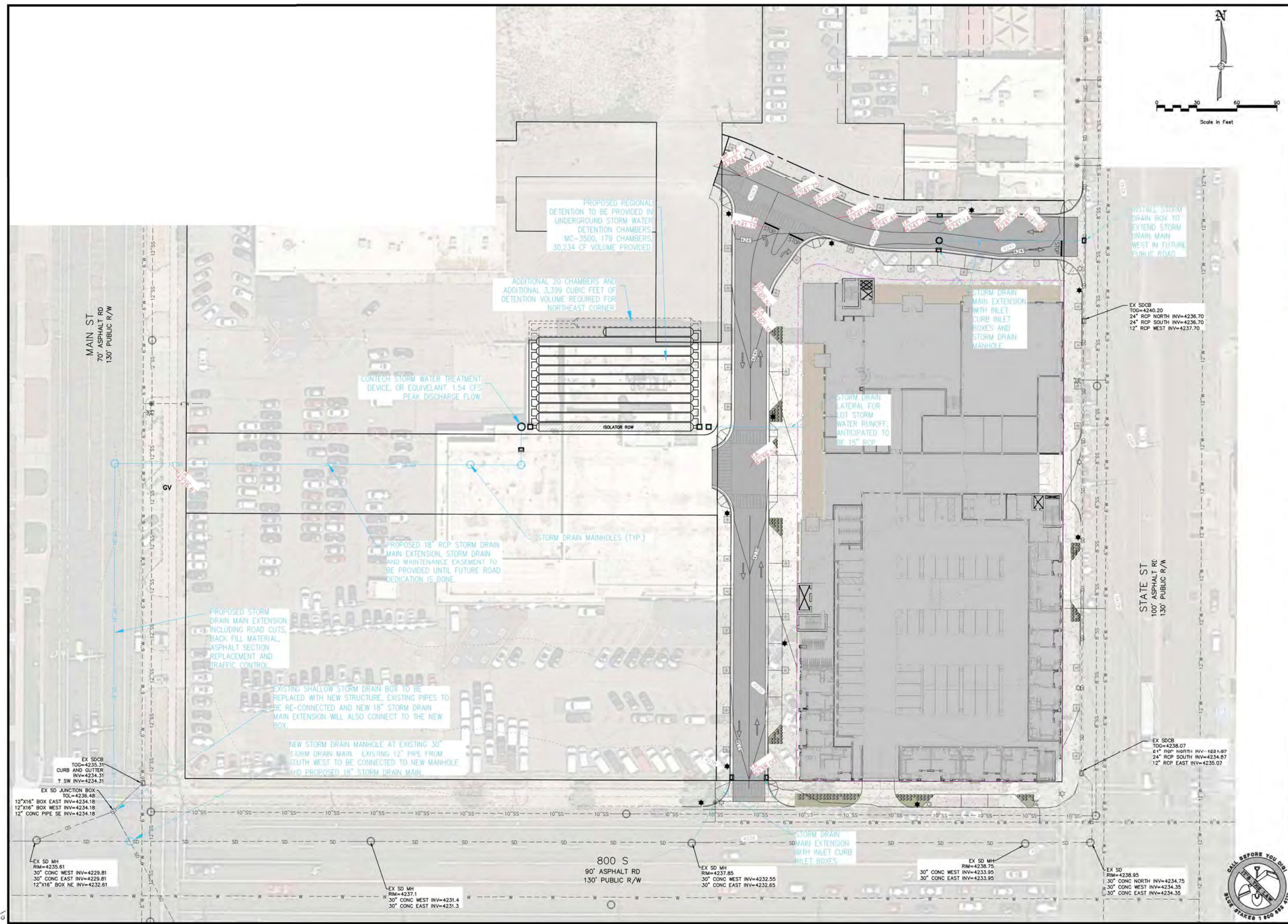
SHEET NO.  
**C0**

PROJECT ID: K1019-01 DATE: 5/28/20  
 FILE NAME: PRJ-SLS SCALE: 1"=50'









NO.	REVISIONS	BY	DATE

**CIVIL ENGINEERING + SURVEYING**  
**GIR**  
 3032 SOUTH 1030 WEST, SUITE 202  
 SLC, Utah 84119 - 801-949-6296  
 DESIGNER: SDT

**SD SET - SEARS BLOCK - PHASE 1**  
 750 SOUTH STATE STREET, SLC, UTAH  
**GRADING & DRAINAGE PLAN**



SHEET NO. **C2**  
 PROJECT ID: K1019-01  
 DATE: 5/31/20  
 FILE NAME: PRJ-SLS  
 SCALE: 1"=30'



EX SDCB  
 TOG=4235.31  
 CURB AND GUTTER  
 INV=4234.31  
 ? SW INV=4234.31

EX SD JUNCTION BOX  
 TOL=4236.48  
 12"x16" BOX EAST INV=4234.18  
 12"x16" BOX WEST INV=4234.18  
 12" CONC PIPE SE INV=4234.18

EX SD MH  
 RIM=4235.61  
 30" CONC WEST INV=4229.81  
 30" CONC EAST INV=4229.81  
 12"x16" BOX NE INV=4232.61

EX SD MH  
 RIM=4237.1  
 30" CONC WEST INV=4231.4  
 30" CONC EAST INV=4231.3

**800 S**  
 90' ASPHALT RD  
 130' PUBLIC R/W

EX SD MH  
 RIM=4237.85  
 30" CONC WEST INV=4232.55  
 30" CONC EAST INV=4232.65

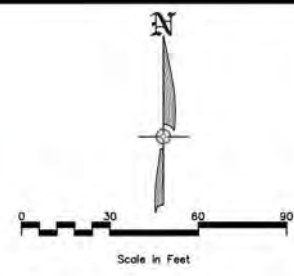
EX SD MH  
 RIM=4238.75  
 30" CONC WEST INV=4233.95  
 30" CONC EAST INV=4233.95

EX SD  
 RIM=4238.95  
 30" CONC NORTH INV=4234.75  
 30" CONC WEST INV=4234.35  
 30" CONC EAST INV=4234.35

EX SDCB  
 TOG=4238.07  
 24" RCP NORTH INV=4231.87  
 24" RCP SOUTH INV=4234.87  
 12" RCP EAST INV=4235.07

EX SDCB  
 TOG=4240.20  
 24" RCP NORTH INV=4236.70  
 24" RCP SOUTH INV=4236.70  
 12" RCP WEST INV=4237.70





NO.	REVISIONS	BY	DATE

**CIVIL ENGINEERING + SURVEYING**  
**GIR**  
 3032 SOUTH 1030 WEST, SUITE 202  
 SLC, Utah 84119 - 801-949-6296  
 DESIGNER: SDT PROJECT ENGINEER: SDT

**SD SET - SEARS BLOCK - PHASE 1**  
 750 SOUTH STATE STREET, SLC, UTAH  
**UTILITY PLAN**



SHEET NO.	C3
PROJECT ID	K1019-01
DATE	5/31/20
FILE NAME	PRJ-SLS
SCALE	1" = 30'



EX SS MH  
 RIM=4237.22  
 10" PVC EAST INV=4230.02  
 10" PVC WEST INV=4229.82  
**800 S**  
 90' ASPHALT RD  
 130' PUBLIC R/W

EX SS MH  
 RIM=4238.93  
 8" PVC NORTH INV=4231.53  
 10" PVC WEST INV=4231.43

STATE ST  
 100' ASPHALT RD  
 130' PUBLIC R/W

EX SS MH  
 RIM=4240.56  
 8" PVC NORTH INV=4233.26  
 8" PVC EAST INV=4233.26